



# THE GOODSYARD

Design Guide - Revision A

May 2020 - Part 3 of 6



ballymore.



## 2.3 INTRODUCTION TO THE SITE-WIDE PARAMETERS

The following section provides an explanation of the content of the parameter plans submitted for approval as part of the amendments to the Outline Planning Application (OPA).

The commentary will provide illustration and descriptions to clarify the content.

The parameter plans set the legal boundaries and scope for the development and record the issues core to the successful delivery of the masterplan in its proposed format. The illustrative masterplan as described in the Design and Access Statement that accompanies this application demonstrates one way in which the development may be delivered within the parameters of the Revised Scheme.

The detail of the outline elements of the Revised Scheme will come forward by Reserved Matters Applications. In this scenario, the Design Guide will act as the reference point for the Reserved Matters Applications.

The parameters define the limits within which the development must be designed. These limits should be read in conjunction with the design guidelines that layer further detailed rules on the illustrative plots.

### 2.3.1 Glossary

### 2.3.2 (A) For approval:

Information for which planning approval is sought in principle: this information controls the detailed design information that will come forwards at a later date as reserved matters applications.

### 2.3.3 (Di) Design information:

Information that is provided for clarity; this information is provided to support the parameters for approval and/or support other sections of the Design and Access statement that accompanies this application.

### 2.3.4 Content of parameter and information plans

The parameter plans will cover the following elements:

- Application Boundary (A)
- Heritage and conservation (Di)
- Constraints (Di)
- Existing levels - ground (Di)
- Existing levels - podium (Di)
- Existing site sections (Di)
- Retentions and Demolitions (A)
- Development plots - basement (A)
- Development plots - ground (A)
- Development plots - podium (A)
- Public realm areas - ground (A)
- Public realm areas - podium (A)
- Pedestrian access - ground (A)
- Pedestrian access - podium (A)
- Servicing and emergency access - ground (A)
- Servicing and emergency access - podium (A)
- Cycles and public transport (Di)
- Maximum parameter heights - Plan (A)
- Maximum parameter heights - Sections (A)

- Phasing - ground (A)
- Phasing - podium (A)
- Frontages and use - ground (A)
- Frontages and use - podium (A)
- Frontages and use - typical upper (A)
- Utilities - ground (A)
- Utilities - podium (A)
- Proposed levels - ground (A)
- Proposed levels - podium (A)

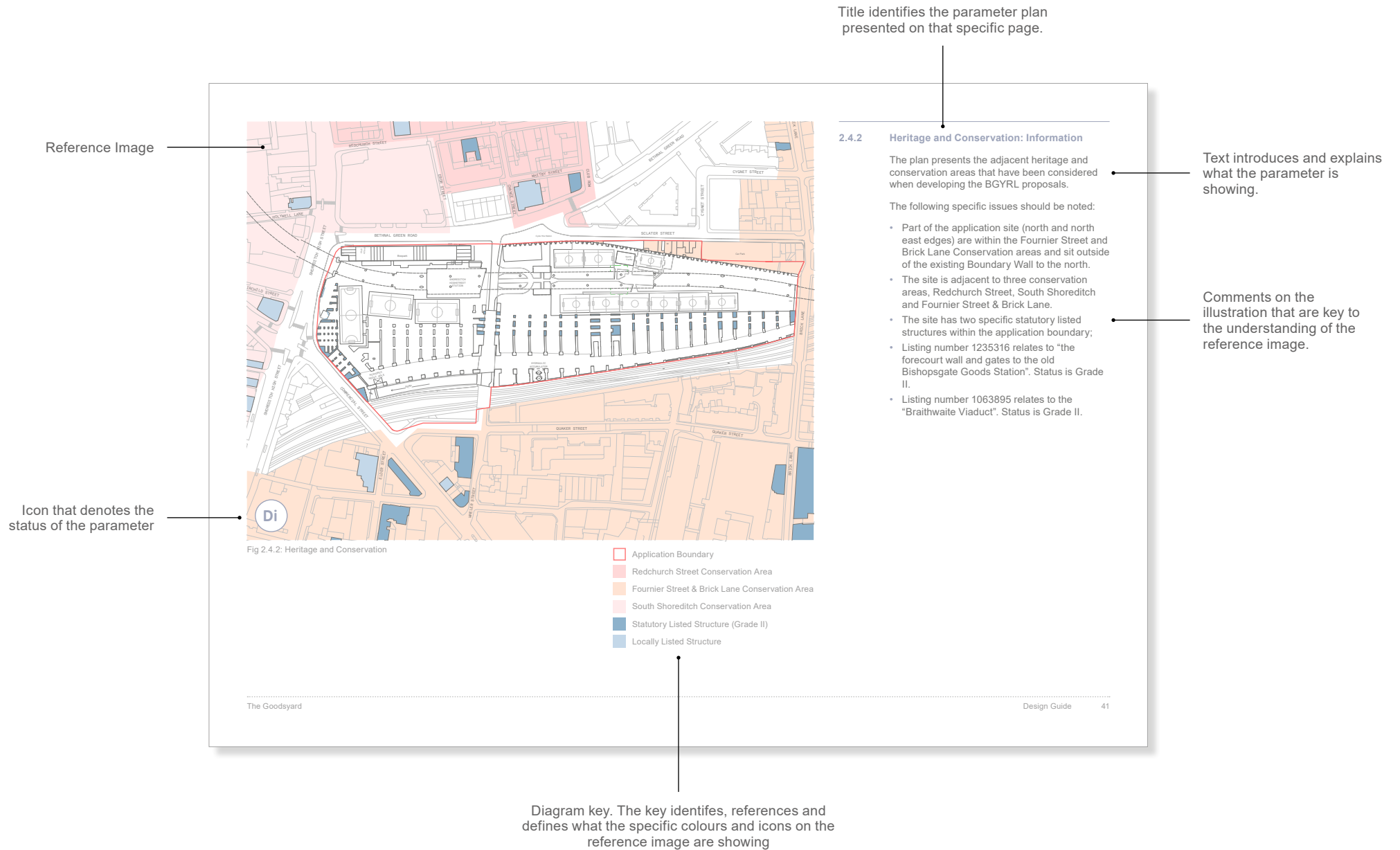
### 2.3.5 Parameter overview

The adjacent extract (Fig 2.3.1) provides an example parameter plan which clarifies and defines specific elements that stakeholders should be aware of.

Note: Content has been deliberately obscured to allow reviewers to establish the format of the page layouts used throughout this section.

Note: All of the parameters illustrated within this section are submitted for approval however some accompanying plans are submitted for information only and are labelled as such (see glossary below).

Fig 2.3.1: Overview example parameter page layout



## 2.4 PARAMETER PLANS

### 2.4.1 Application Boundary: For Approval

The adjacent plan identifies the proposed application boundary for the Bishopsgate Goodsyrd planning application. This drawing has not been amended since the original scheme for the site.

Application boundary area = 4.4Ha / 10.9 acres

The following specific issues should be noted:

- Boundary extends to back of pavement on Shoreditch High Street.
- Boundary extends to back of pavement on Bethnal Green Road/Sclater Street.
- Boundary extends to back of pavement along Brick Lane.
- Boundary is de-marked by the existing historic structure along the southern boundary.
- Boundary extends beyond footpath at Quaker Street to allow for realignment of the highway and associated works to the footpath.
- Boundary includes Wheler Street and Braithwaite Street that runs through the centre of the site.

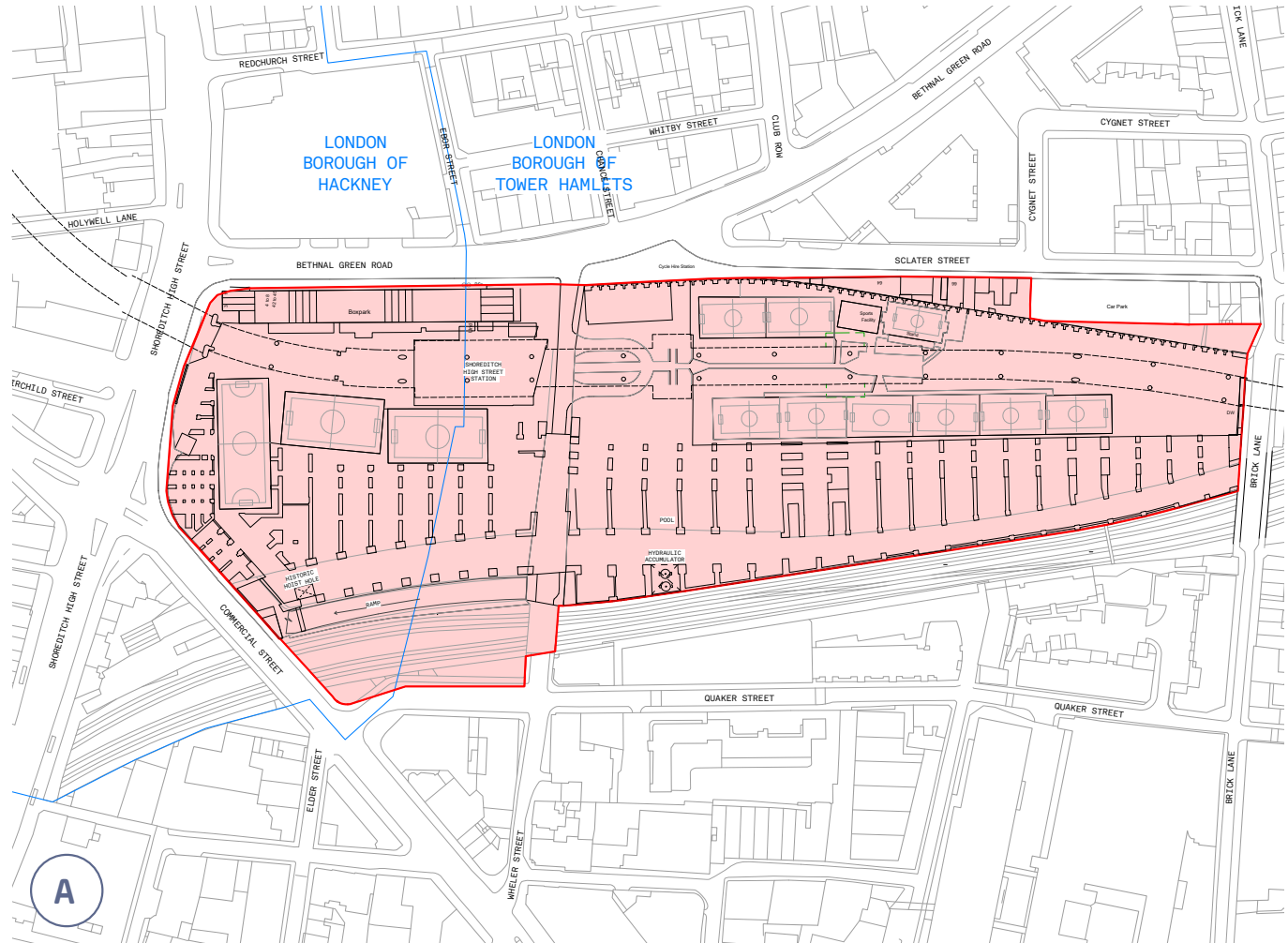


Fig 2.4.1: Application Boundary

□ Planning Application Boundary  
— London Borough Boundary

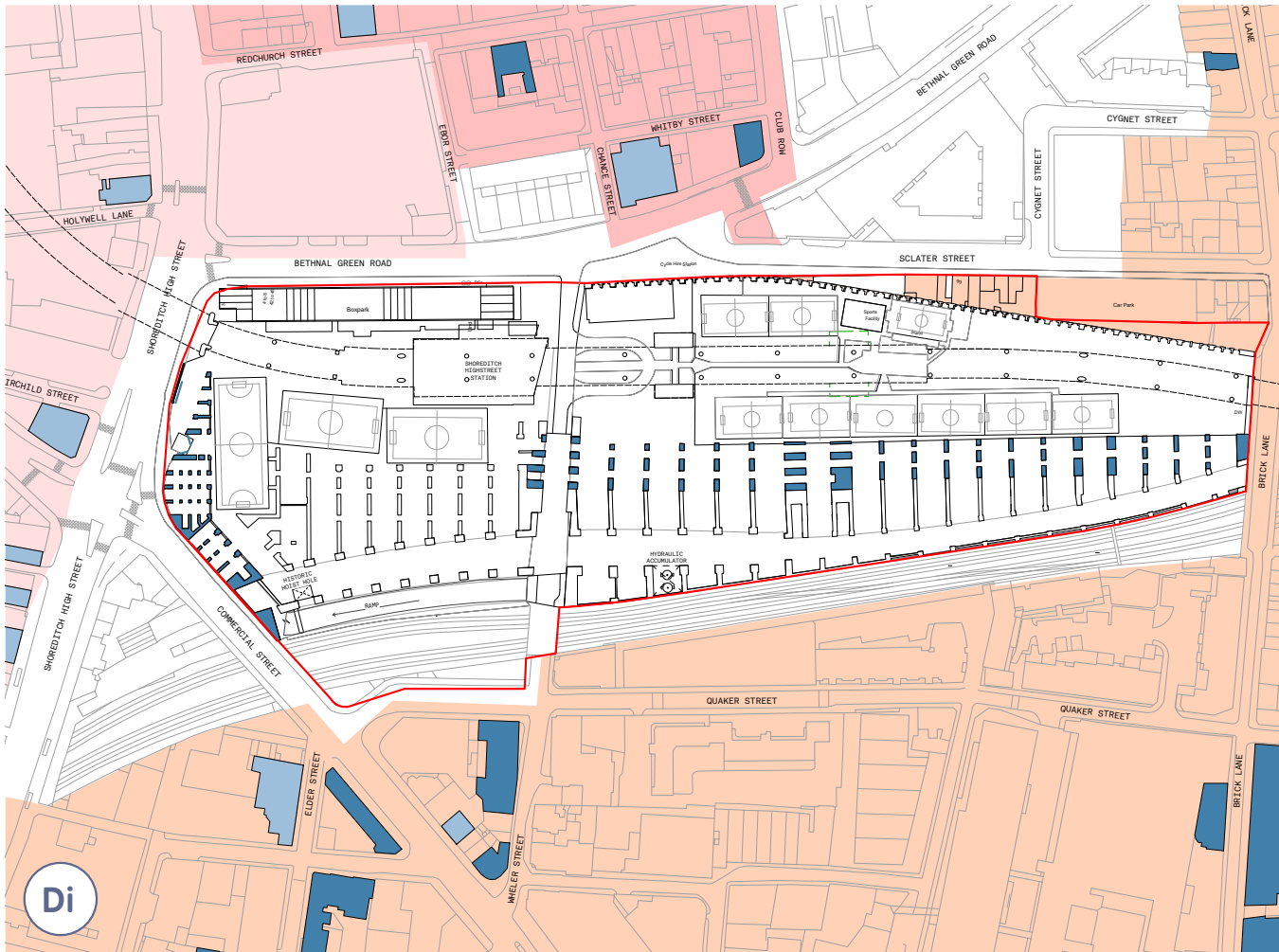


Fig 2.4.2: Heritage and Conservation

- Application Boundary
- Redchurch Street Conservation Area
- Fournier Street & Brick Lane Conservation Area
- South Shoreditch Conservation Area
- Statutory Listed Structure (Grade II)
- Locally Listed Structure

## 2.4.2 Heritage and Conservation: Information

The plan presents the adjacent heritage and conservation areas that have been considered when developing the BGYRL proposals.

The following specific issues should be noted:

- Part of the application site (north and north east edges) are within the Fournier Street and Brick Lane Conservation areas and sit outside of the existing Boundary Wall to the north.
- The site is adjacent to three conservation areas, Redchurch Street, South Shoreditch and Fournier Street & Brick Lane.
- The site has two specific statutory listed structures within the application boundary;
- Listing number 1235316 relates to “the forecourt wall and gates to the old Bishopsgate Goods Station”. Status is Grade II.
- Listing number 1063895 relates to the “Braithwaite Viaduct”. Status is Grade II.

### 2.4.3 Site Constraints: Information

The plan presents the strategic constraints that have shaped the Revised Scheme, and the proposals for the development of each plot. It is important that these are recorded as the detail design for each plot will be required to give consideration to these.

It should be noted that each Plot (detailed and Illustrative) will individually assess the immediate site constraints relative to that specific plot, if a constraint is not identified in the site-wide parameters.

The following specific issues should be noted:

- The underground constraints (8 track reserve, suburban rail line, Overground foundation zone, Underground Central Line and BT Tunnels) represent significant constraints for 'founding' buildings.
- Both the Westminster Pier and King Henry VII LVMF views are assessed in detail in the Environmental Statement, Townscape and Visual Impact Assessment and represent height restrictions.
- Listed and Non-listed heritage assets are identified, these elements should be cross-referenced against the retention and demolition parameters.

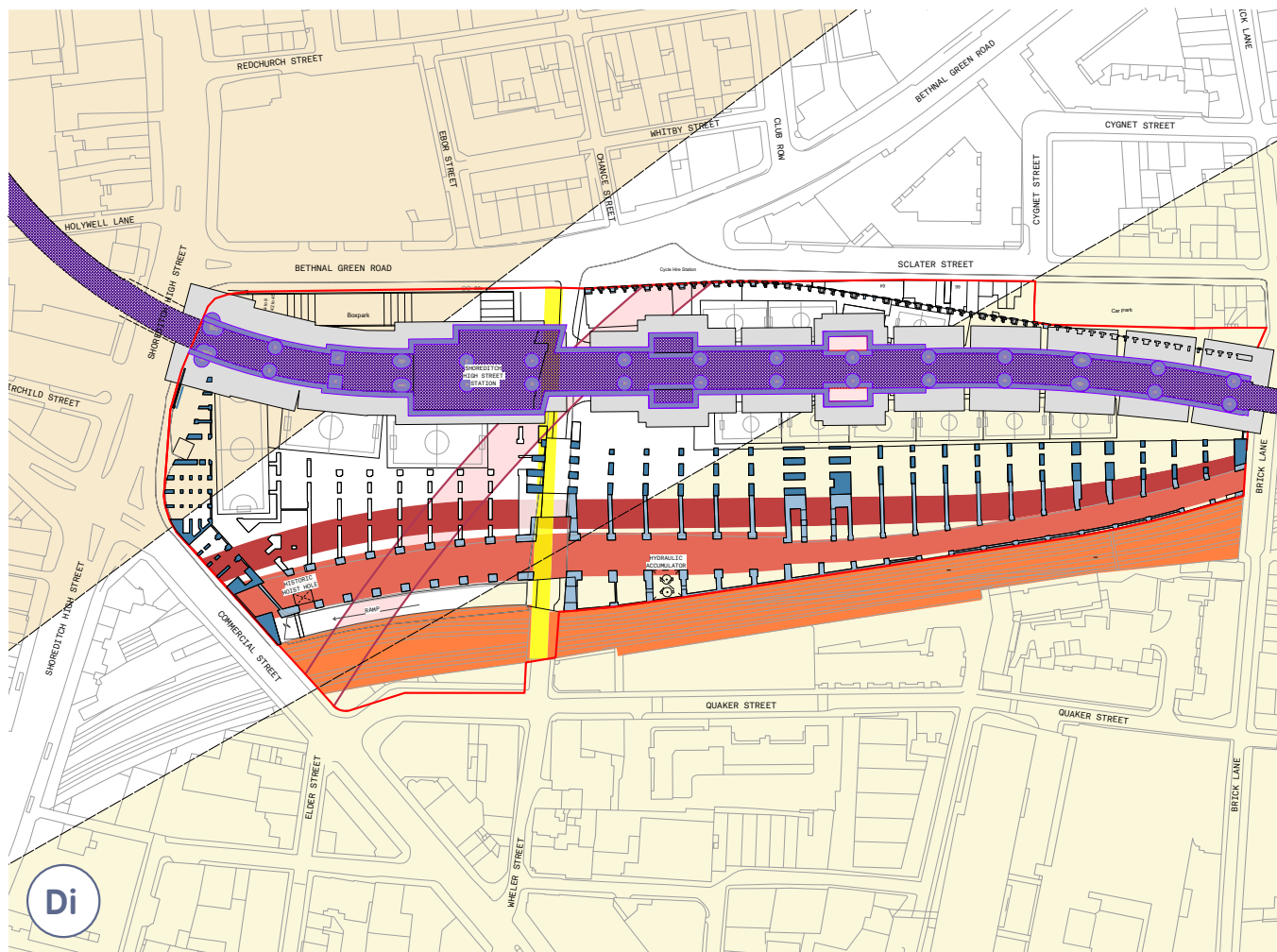
















Fig 2.4.3: Site Constraints

	Application Boundary		London Overground 2m Inspection Zone
	BT Tunnel		Zone Reserved for Expansion
	Main Line		London Overground Foundation Zone
	Suburban Rail Line		Grade II Listed Heritage Asset
	8 Track Reserve		Non-Listed Heritage Asset
	Underground (Central Line)		Westminster Pier LVMF View Cone
	London Overground Rail Line		King Henry VII's Mound LVMF View Cone



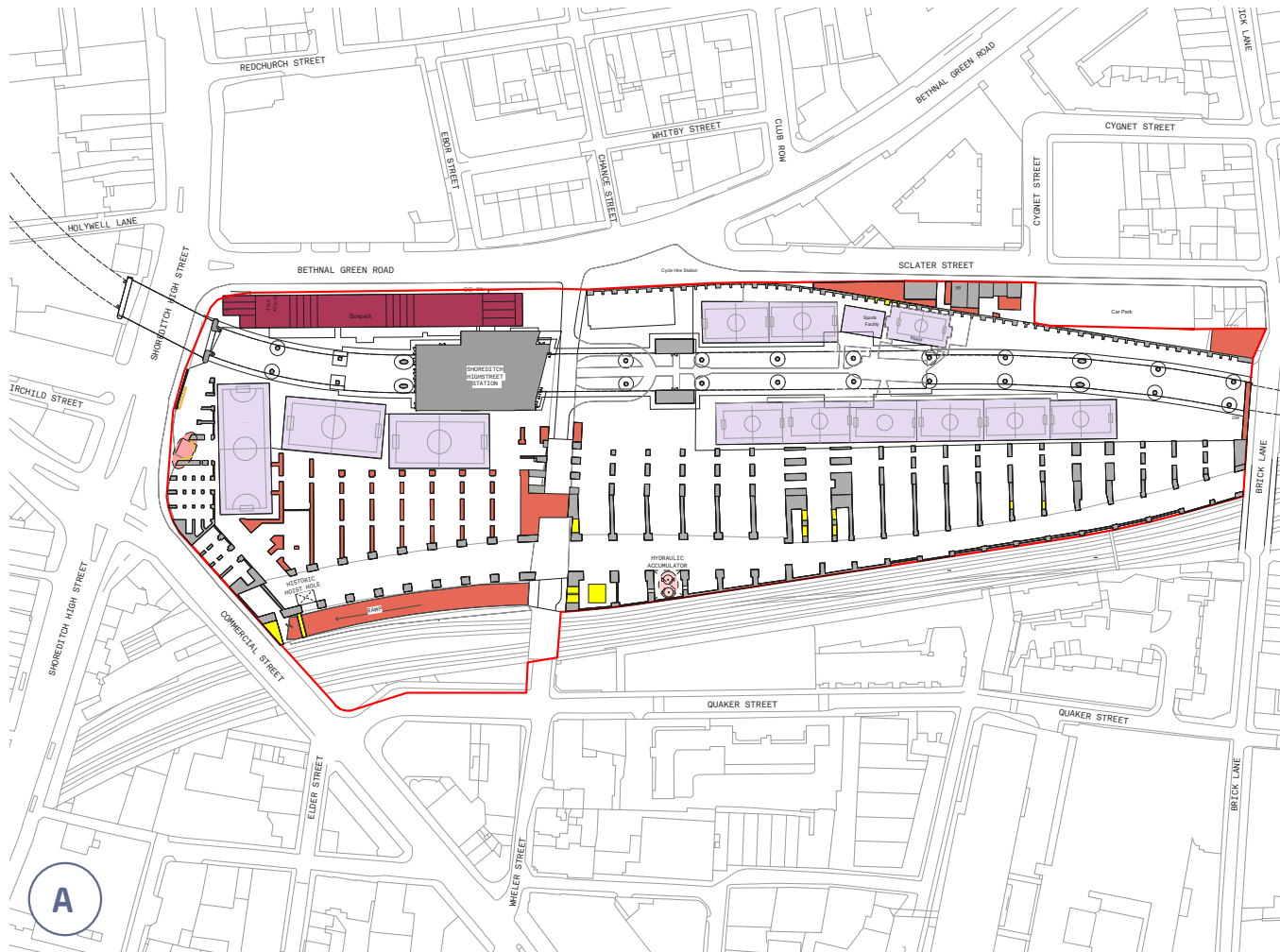
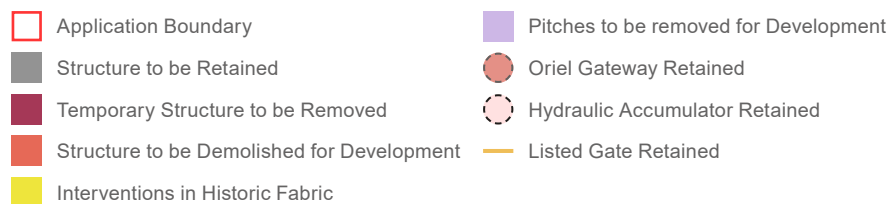


Fig 2.4.4: Retentions and Demolitions



## 2.4.4 Retentions and Demolitions: For Approval

The plan presents the structures within the application boundary that are to be retained and demolished to allow the proposed development to take place.

The parameter plan identifies both permanent and temporary structures.

The following specific issues should be noted:

- Some of the arches (south of the Overground and north of London Road) need to be demolished to allow for development.
- Temporary uses and structures such as Box Park and the football pitches will need to be removed.
- The pump rooms (south of London Road) are to be retained.
- The Oriel Gateway is to be retained.
- The full extent of the Grade II listed structures are to be retained and refurbished where required.
- This parameter should be read in conjunction with the submitted application drawing BGY-FBA-00-XX-DR-A-05\_10\_012 "Demolition plan - heritage assets".

## 2.4.5 Amendments

Dialogue with TfL and the Boroughs highways officers has resulted in amendments to allow for safe vehicular access and egress from the service yard between Plots 2 and 8. These changes are reflected on the retentions and demolition plan (Fig 2.4.4).

## 2.4.6 Existing Site Levels - Ground: For Information

The plan presents the existing ground levels, both within and immediately outside the application boundary.

The levels presented on the plan represent a summary of the site wide topographical survey.

The following specific issues should be noted:

- Levels indicated in red are measured surveyed levels within the application boundary.
- Levels indicated in black measured surveyed levels outside the application boundary.
- Levels indicated in blue are unverified levels extrapolated from a 3D arch survey.
- Existing and proposed parameter section lines are denoted by the blue dashed lines.
- All spot levels are measured in metres above ordnance datum (AOD).

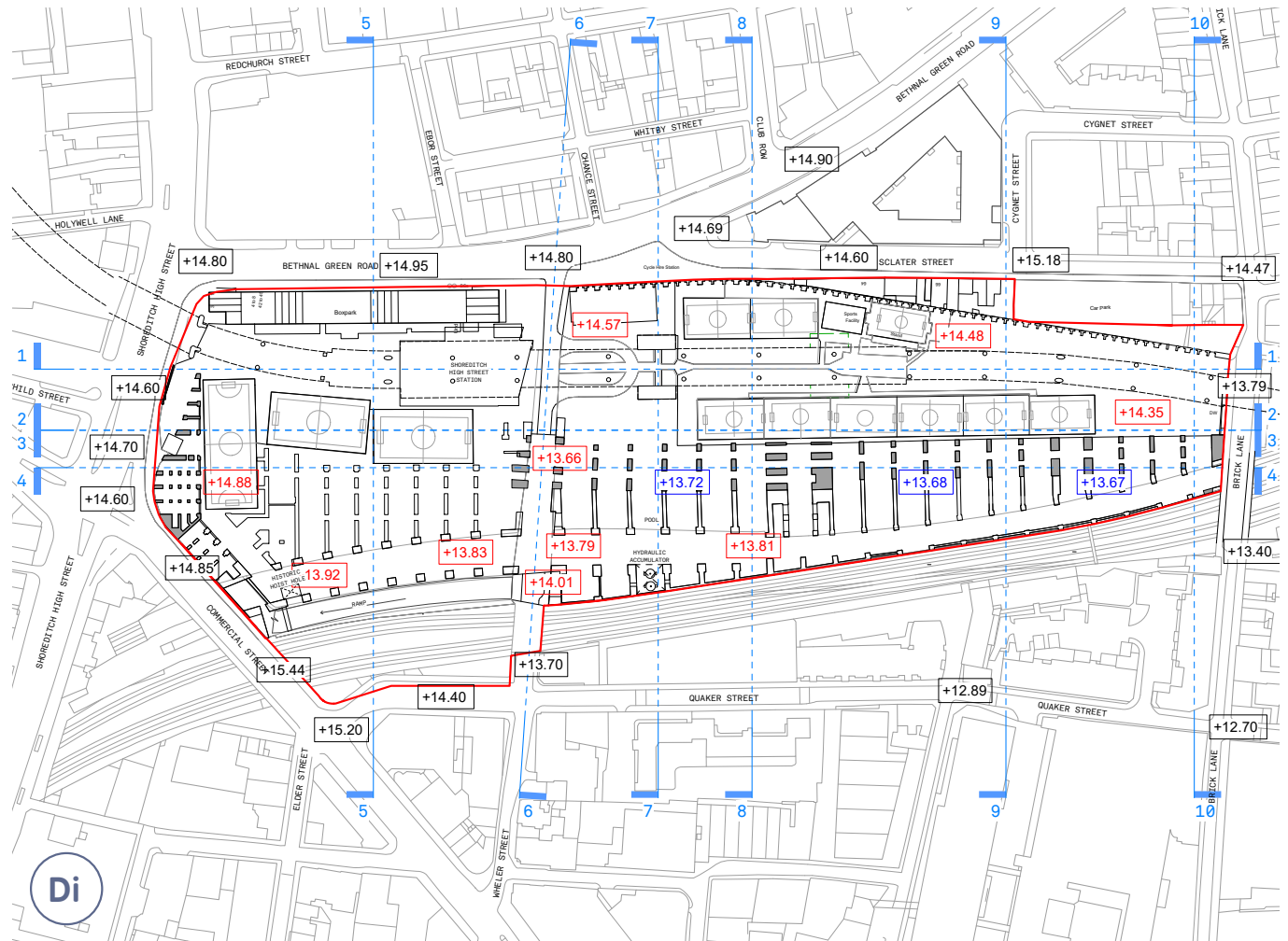


Fig 2.4.5: Existing Site Levels - Ground

- Application Boundary
- +15.00 Measured Levels Inside Site Boundary (Metres A.O.D)
- +15.00 Measured Levels Outside Site Boundary (Metres A.O.D)
- +15.00 Unverified Levels Inside Site Boundary (Metres A.O.D)  
Extrapolated from Plowman Craven 3D Arch Survey: W15532-010SM



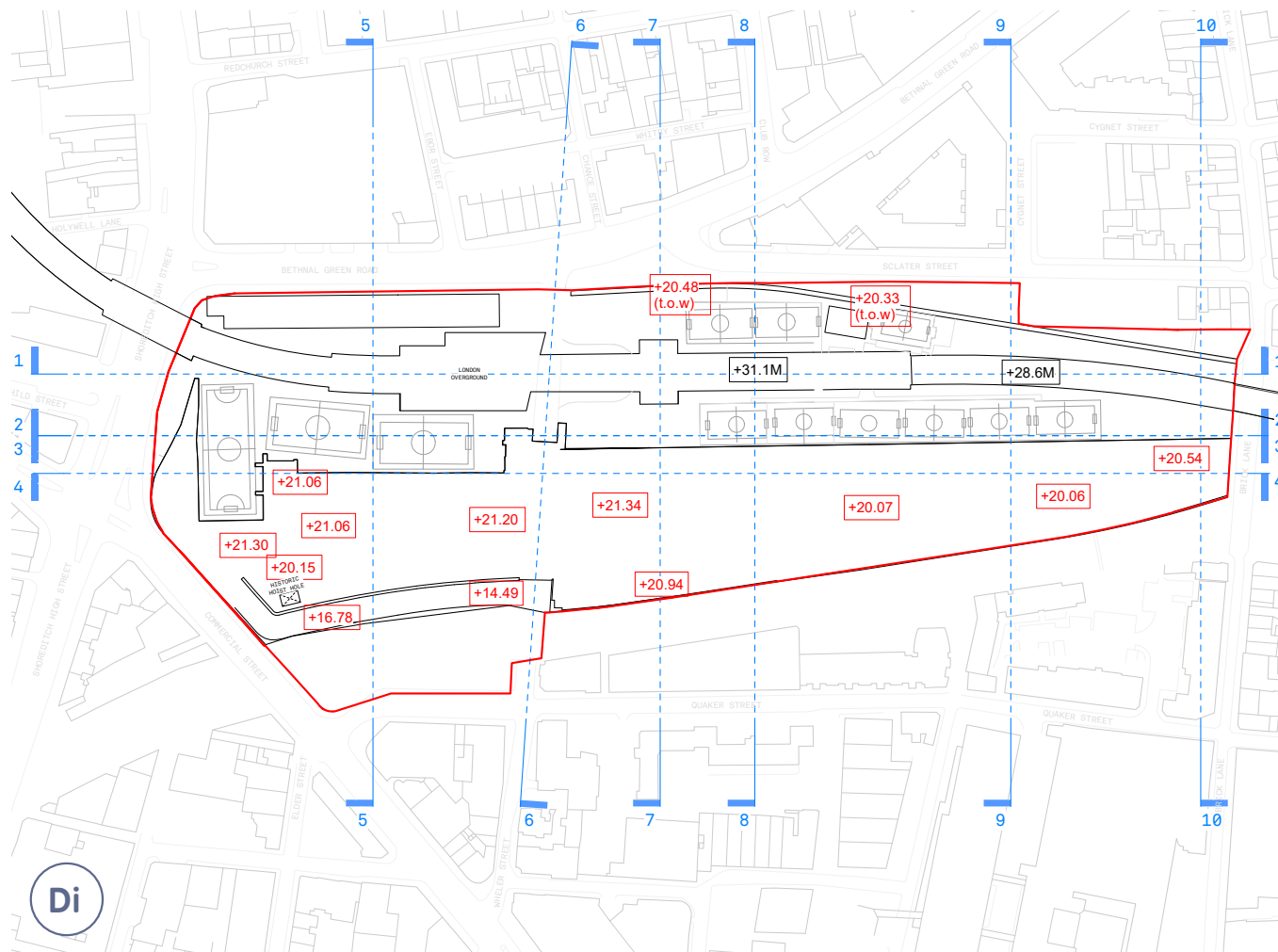


Fig 2.4.6: Existing Site Levels - Platform

- Application Boundary
- +15.00 Measured Levels Inside Site Boundary (Metres A.O.D)
- +15.00 Levels of London Overground
- t.o.w Top of Wall

## 2.4.7 Existing Site Levels - Platform: For Information

The plan presents the existing platform levels, within the application boundary.

The levels presented on the plan represent a summary of the site wide topographical survey.

The following specific issues should be noted:

- Levels indicated in red are measured surveyed levels within the application boundary.
- Levels indicated in black measured surveyed levels outside the application boundary.
- Existing and proposed parameter section lines are denoted by the blue dashed lines.

All spot levels are measured in metres above ordnance datum (AOD).

## 2.4.8 Development Plots - Basement: For Approval

The plan presents the maximum plot extent at basement level.

Plot 2 and 7(a-d) are excluded from these parameters as their details are being submitted in full within the application.

The following specific issues should be noted:

- The Plot is defined as building development area exclusive of balconies and private space at adjacent grade.
- Building footprints exist within the plot boundaries and are defined and described within the maximum and minimum parameters and individual plot design guides.

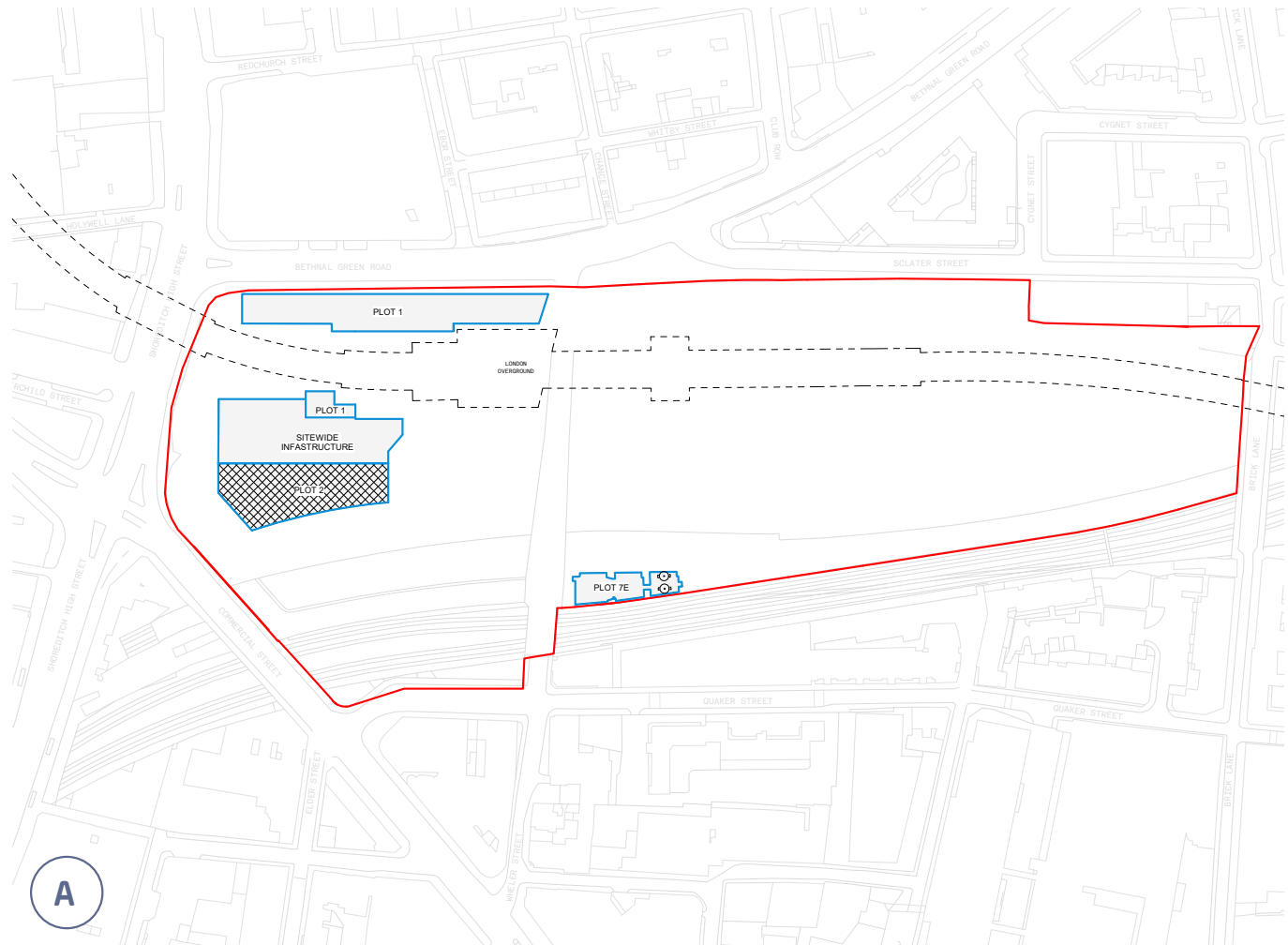


Fig 2.4.7: Development Plots - Basement

- Application Boundary
- Building Extents Above (Projection)
- Building Plots (Maximum Extents)
- - London Overground Line
- ▣ Full Details Submitted

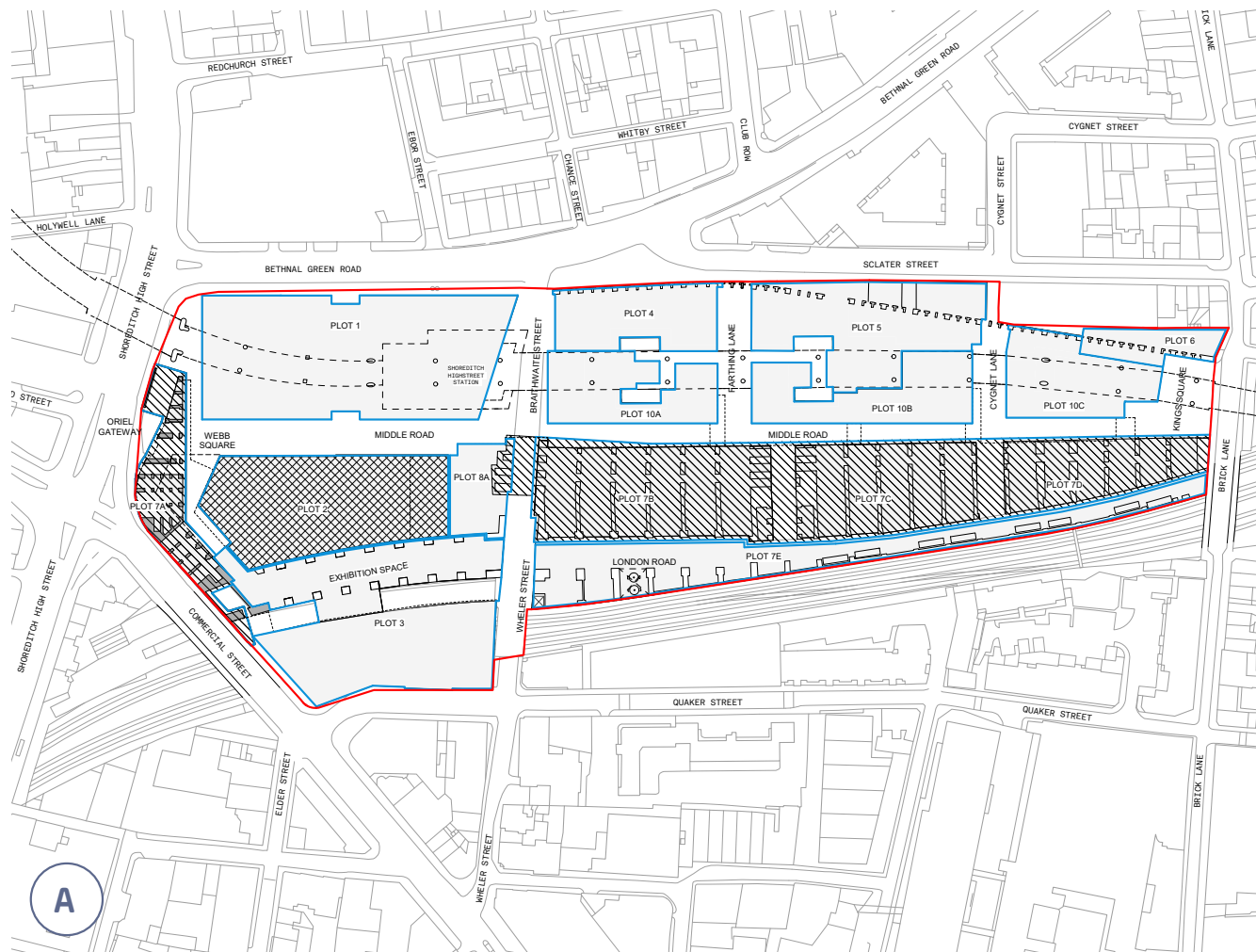


Fig 2.4.8: Development Plots - Ground

- Application Boundary
- Building Extents Above (Projection)
- Building Plots (Maximum Extents)
- London Overground Line
- Full Details Submitted
- Full Details Submitted and LBA

## 2.4.9 Development Plots - Ground: For Approval

The plan presents the maximum plot extent at ground level.

Plot 2 and 7(a-d) are excluded from these parameters as their details are being submitted in full within the application.

The following specific issues should be noted:

- The Plot is defined as building development area exclusive of balconies and private space at adjacent grade.
- Building footprints exist within the plot boundaries and are defined and described within the maximum and minimum parameters and individual plot design guides.
- The London Overground has a reserved 2m inspection zone around the existing structures which must be adhered to. The plots at ground floor level run into the space underneath the overground viaduct but building development should allow for access and escape as described in the plot design guides.

## 2.4.10 Development Plots - Platform: For Approval

The plan presents the maximum plot extent at platform level.

Plot 2 is excluded from these parameters as the details are being submitted in full within the application.

The following specific issues should be noted:

- The Plot is defined as building development area exclusive of balconies and private space at adjacent grade.
- Building footprints exist within the plot boundaries and are defined and described within the maximum and minimum parameters and individual plot design guides.

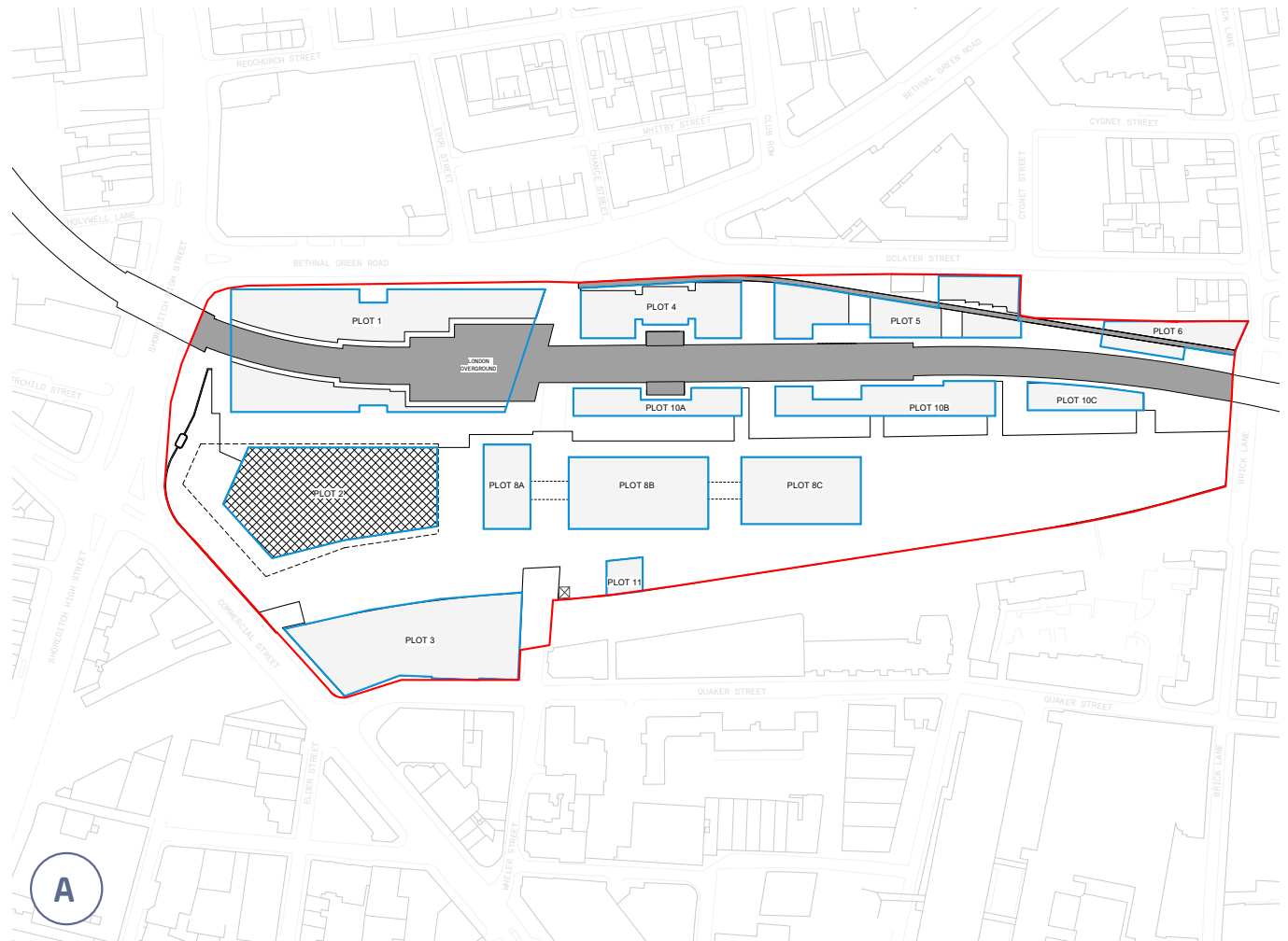


Fig 2.4.9: Development Plots - Platform

- Application Boundary
- Building Extents Above (Projection)
- Building Plots (Maximum Extents)
- - London Overground Line
- ▣ Full Details Submitted

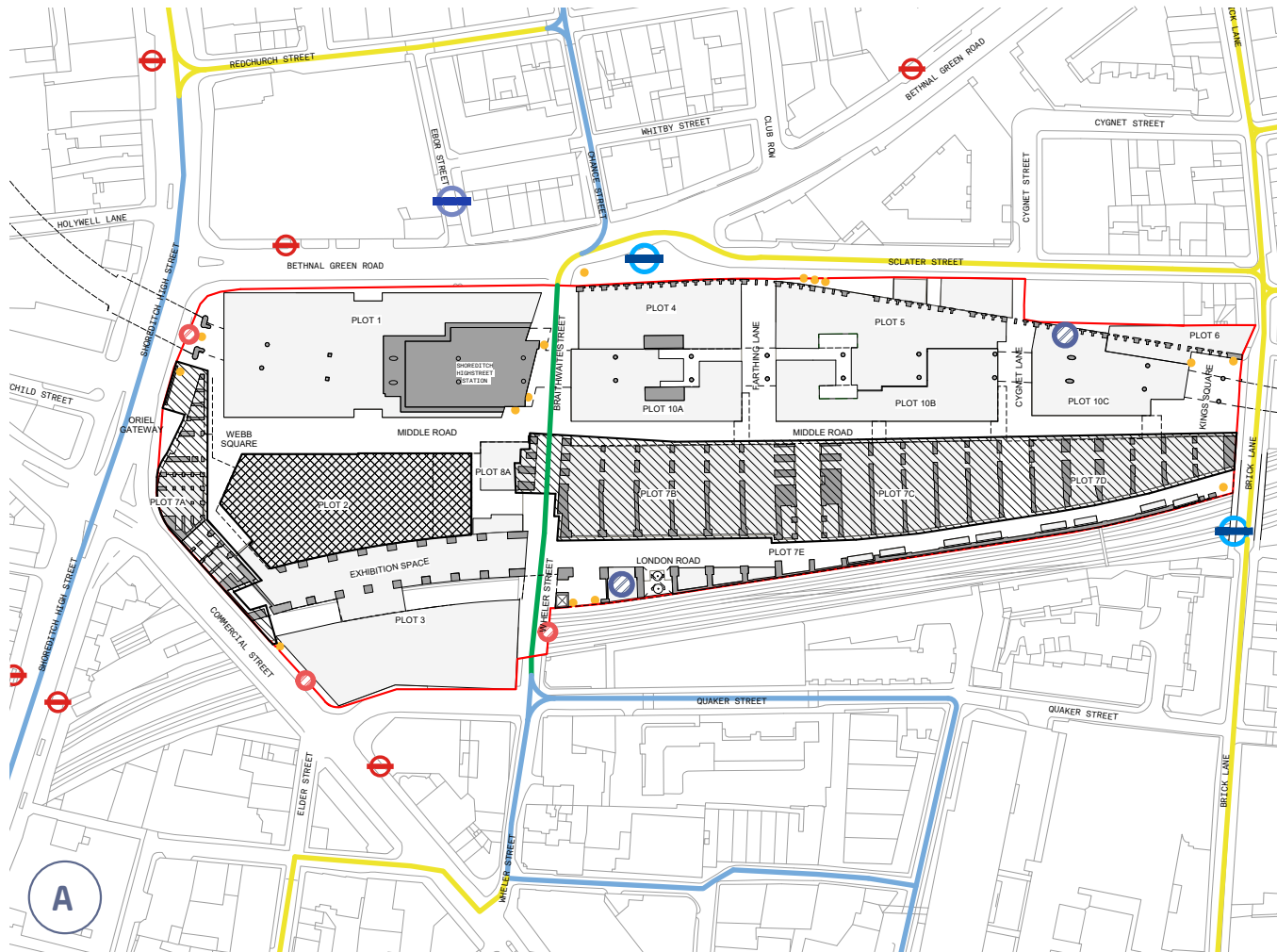
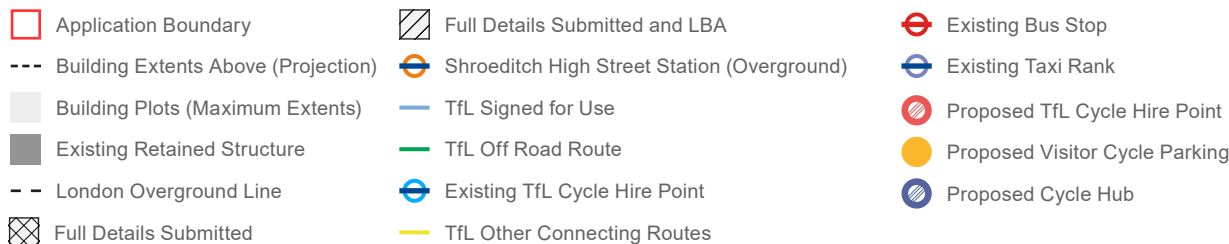


Fig 2.4.10: Access - Public Transport and Cycles



## 2.4.11 Access - Public Transport and Cycles: For Approval

The plan presents the existing and proposed public transport and cycle infrastructure on and around the site.

Public transport indicated on the plan is as follows:

- London Overground Train
- Bus
- Taxi

Full details of the transport proposals can be reviewed in the Transport assessment that accompanies this application.

The following specific issues should be noted:

- The existing "off-road route" as identified in the TfL's 'Local Cycling Guide' will be maintained and upgraded.
- Cycle parking will be provided within the proposed development in accordance with the ratios set out in the Development Specification.
- Two secure managed cycle hubs are proposed.
- All on-street cycling provision will be provided in convenient threshold locations and will be in well-lit, well-overlooked areas.



## 2.4.12 Servicing and Emergency Access - Ground: For Approval

The plan presents the proposed principles for service vehicle and emergency vehicle access at ground level.

For more detail see the transport assessment that accompanies the revised scheme submission.

The following specific issues should be noted:

- Car free, policy compliant with draft London Plan.
- Accessible parking (3%) to be met on street.
- All site servicing is provided via the three indicated managed service yards. Trolled servicing is in the positions indicated. Managed out of hours on site servicing to Plot 7, Plot 3 and part Plot 5 (Weavers Cottages and Victorian Building) will be serviced from street.
- Emergency Access to site via Bethnal Green Road, Brick Lane and Wheler Street.
- Access routes for emergency vehicles will be a minimum of 3.7m. Emergency access through plot 2/8 service yard is to be provided to allow for resilience.
- All new access point will be subject to a safety audit.

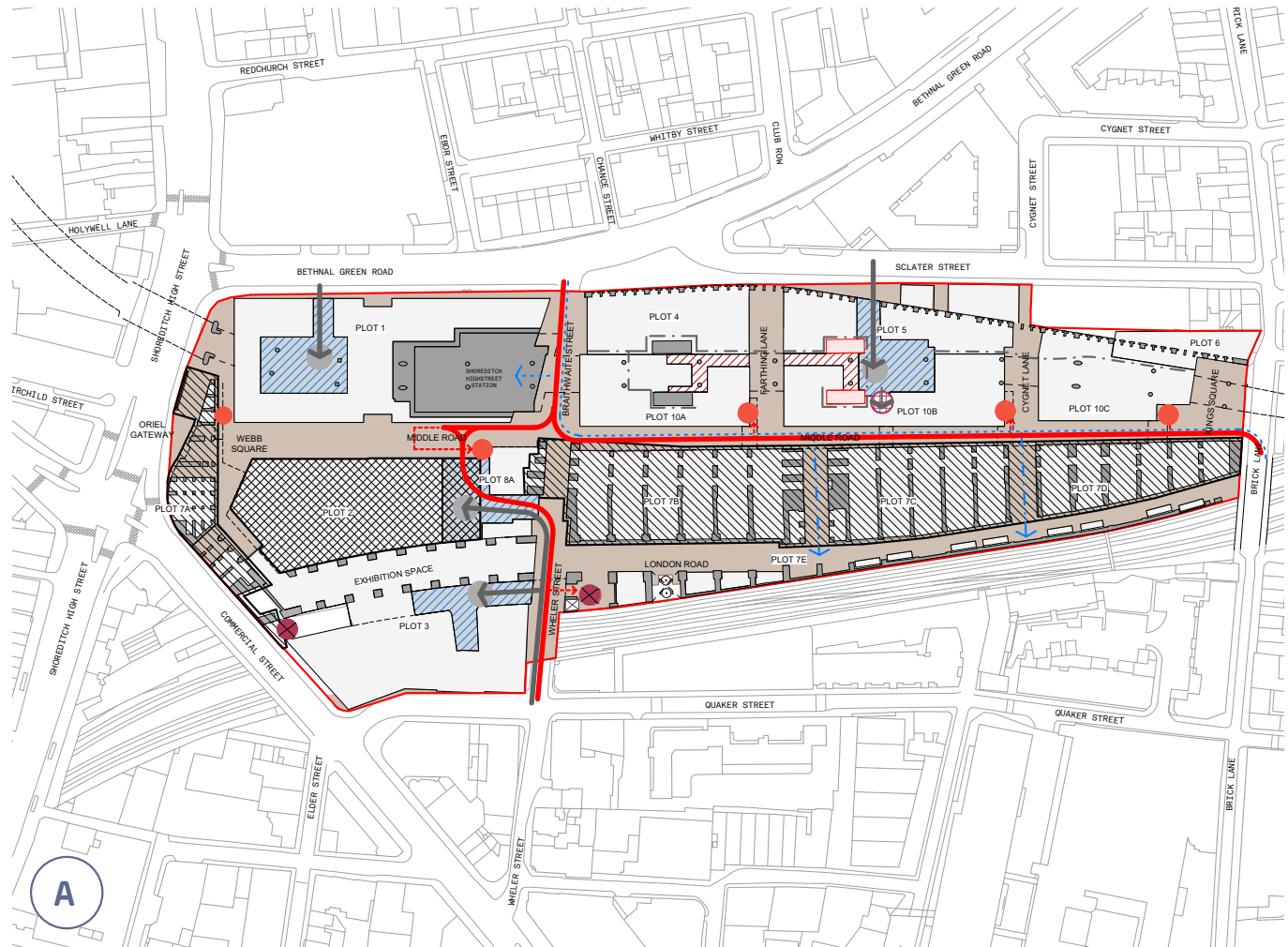


Fig 2.4.11: Servicing and Emergency Access - Ground

<span style="border: 1px solid red; display: inline-block; width: 15px; height: 10px;"></span> Application Boundary	<span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); display: inline-block; width: 15px; height: 10px;"></span> Full Details Submitted	<span style="background: radial-gradient(circle, black 1px, transparent 1px); background-size: 4px 4px; display: inline-block; width: 15px; height: 10px;"></span> Primary Service Access	<span style="color: blue;">....</span> Out of Hours (Managed) Service Access
<span style="border: 1px dashed black; display: inline-block; width: 15px; height: 10px;"></span> Building Extents Above (Projection)	<span style="background: repeating-linear-gradient(-45deg, transparent, transparent 2px, black 2px, black 4px); display: inline-block; width: 15px; height: 10px;"></span> Full Details Submitted and LBA	<span style="background-color: #d3d3d3; display: inline-block; width: 15px; height: 10px;"></span> Principal Public Realm Area	<span style="color: blue;">---</span> Out of Hours (Managed) Service Access Trolled
<span style="background-color: #d3d3d3; display: inline-block; width: 15px; height: 10px;"></span> Building Plots (Maximum Extents)	<span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, blue 2px, blue 4px); display: inline-block; width: 15px; height: 10px;"></span> Servicing	<span style="background-color: #ff0000; display: inline-block; width: 15px; height: 10px;"></span> Vertical Circulation	
<span style="background-color: #808080; display: inline-block; width: 15px; height: 10px;"></span> Existing Retained Structure	<span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, red 2px, red 4px); display: inline-block; width: 15px; height: 10px;"></span> London Overground Emergency Exit Route	<span style="background: radial-gradient(circle, black 1px, transparent 1px); background-size: 4px 4px; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> Vertical Circulation (Gated)	
<span style="border: 1px dashed black; display: inline-block; width: 15px; height: 10px;"></span> London Overground Line	<span style="border-bottom: 2px solid red; display: inline-block; width: 15px;"></span> Emergency Vehicle Access	<span style="background: radial-gradient(circle, black 1px, transparent 1px); background-size: 4px 4px; border: 1px solid black; border-radius: 50%; display: inline-block; width: 15px; height: 10px;"></span> Vertical Vehicle Service Access	
<span style="background-color: #ffcccc; display: inline-block; width: 15px; height: 10px;"></span> Zone Reserved for TfL Expansion	<span style="border-bottom: 2px dashed red; display: inline-block; width: 15px;"></span> Fire Access (On Foot)	<span style="border-bottom: 2px dashed black; display: inline-block; width: 15px;"></span> Trolled Service Access	

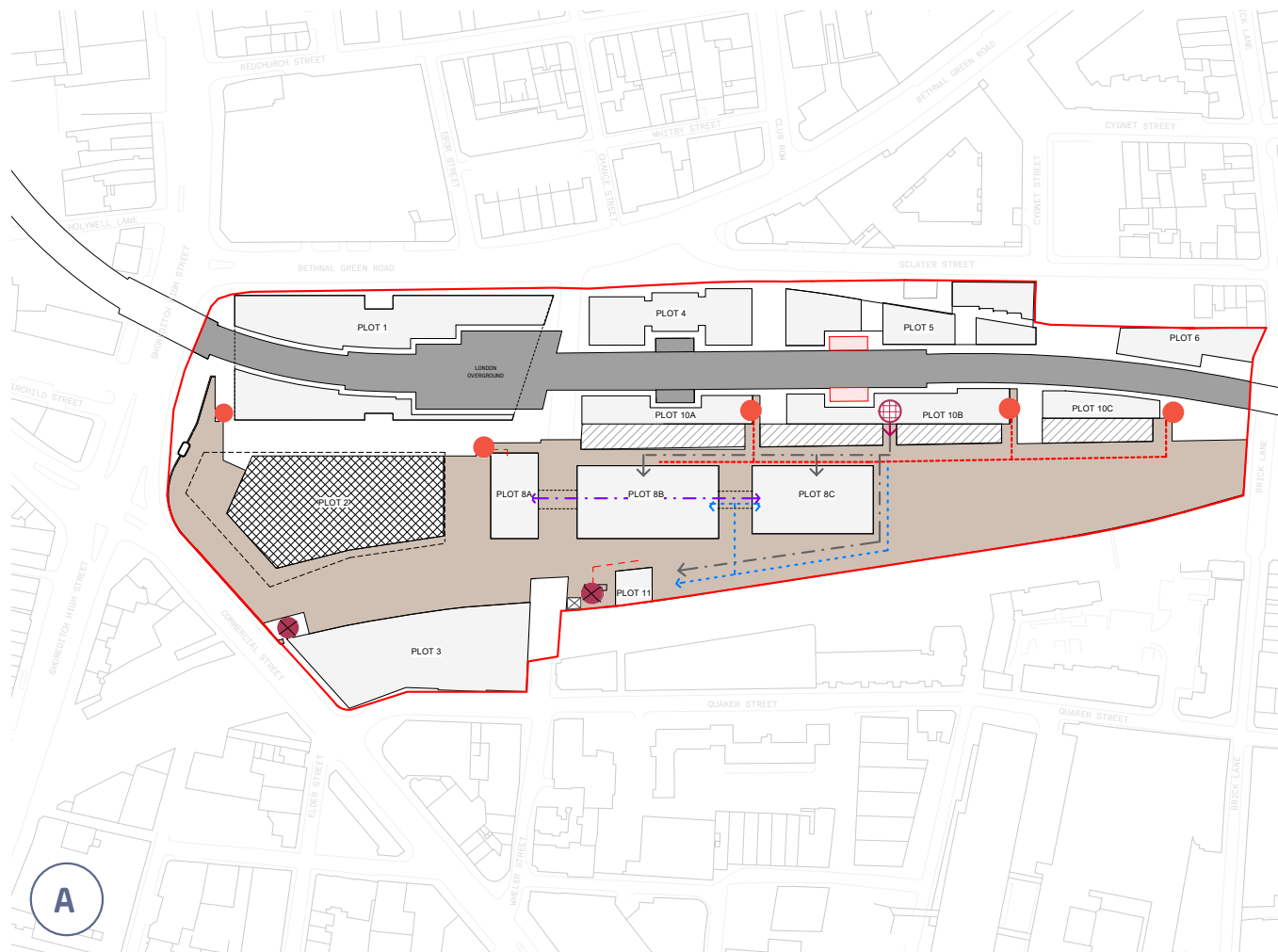


Fig 2.4.12: Servicing and Emergency Access - Platform

<span style="border: 1px solid red; display: inline-block; width: 15px; height: 10px;"></span> Application Boundary	<span style="background-color: #a08060; display: inline-block; width: 15px; height: 10px;"></span> Principal Public Realm Area	<span style="color: red; font-weight: bold;">---</span> Fire Access (On Foot)
<span style="border-top: 1px dashed black; display: inline-block; width: 15px; height: 10px;"></span> Building Extents Above (Projection)	<span style="color: red; font-size: 1.2em;">●</span> Vertical Circulation	<span style="color: blue; font-weight: bold;">....</span> Out of Hours (Managed) Service Access
<span style="background-color: #d3d3d3; display: inline-block; width: 15px; height: 10px;"></span> Building Plots (Maximum Extents)	<span style="color: red; font-size: 1.2em;">⊗</span> Vertical Circulation (Gated)	<span style="color: purple; font-weight: bold;">---</span> Undercroft Service Route
<span style="background-color: #808080; display: inline-block; width: 15px; height: 10px;"></span> Existing Retained Structure	<span style="color: red; font-size: 1.2em;">⊕</span> Vertical Vehicle Service Access	<span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); display: inline-block; width: 15px; height: 10px;"></span> Zone for Horizontal Bridges (Max Width 3m)
<span style="background-color: #ffb6c1; display: inline-block; width: 15px; height: 10px;"></span> Zone Reserved for TfL Expansion	<span style="color: black; font-weight: bold;">---</span> Trollied Service Access	
<span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); display: inline-block; width: 15px; height: 10px;"></span> Full Details Submitted	<span style="color: red; font-weight: bold;">---</span> Emergency Vehicle Access	

## 2.4.13 Servicing and Emergency Access - Platform: For Approval

The plan presents the proposed principles for service vehicle and emergency vehicle access at platform level.

For more detail see the transport assessment that accompanies the revised scheme submission.

The following specific issues should be noted:

- Infrequent vehicle service/maintenance access is provided via vehicle lift which is positioned to the south of the service yard adjacent to Plot 5. An example would be facade maintenance (mobile working platform)
- As no direct fire-fighting vehicle access is provided, platform buildings will be accessed via external stairs as indicated. Buildings at platform level with no core to ground will require wet riser systems and sprinkler systems.

## 2.4.14 Minimum public realm areas - Ground: For Approval

The plan presents the proposed minimum areas of public realm ground level within the site boundary.

Public realm is commonly defined as any space that is free and open to everyone. The London Plan describes it as ‘the space between and within buildings that is publicly accessible, including streets, squares, forecourts, parks and open spaces.’

The following specific issues should be noted:

- Broadly half of the public realm at ground level is under the Grade II listed Braithwaite arches, bringing these back into use.
- Seven access points provide vertical circulation to platform level.

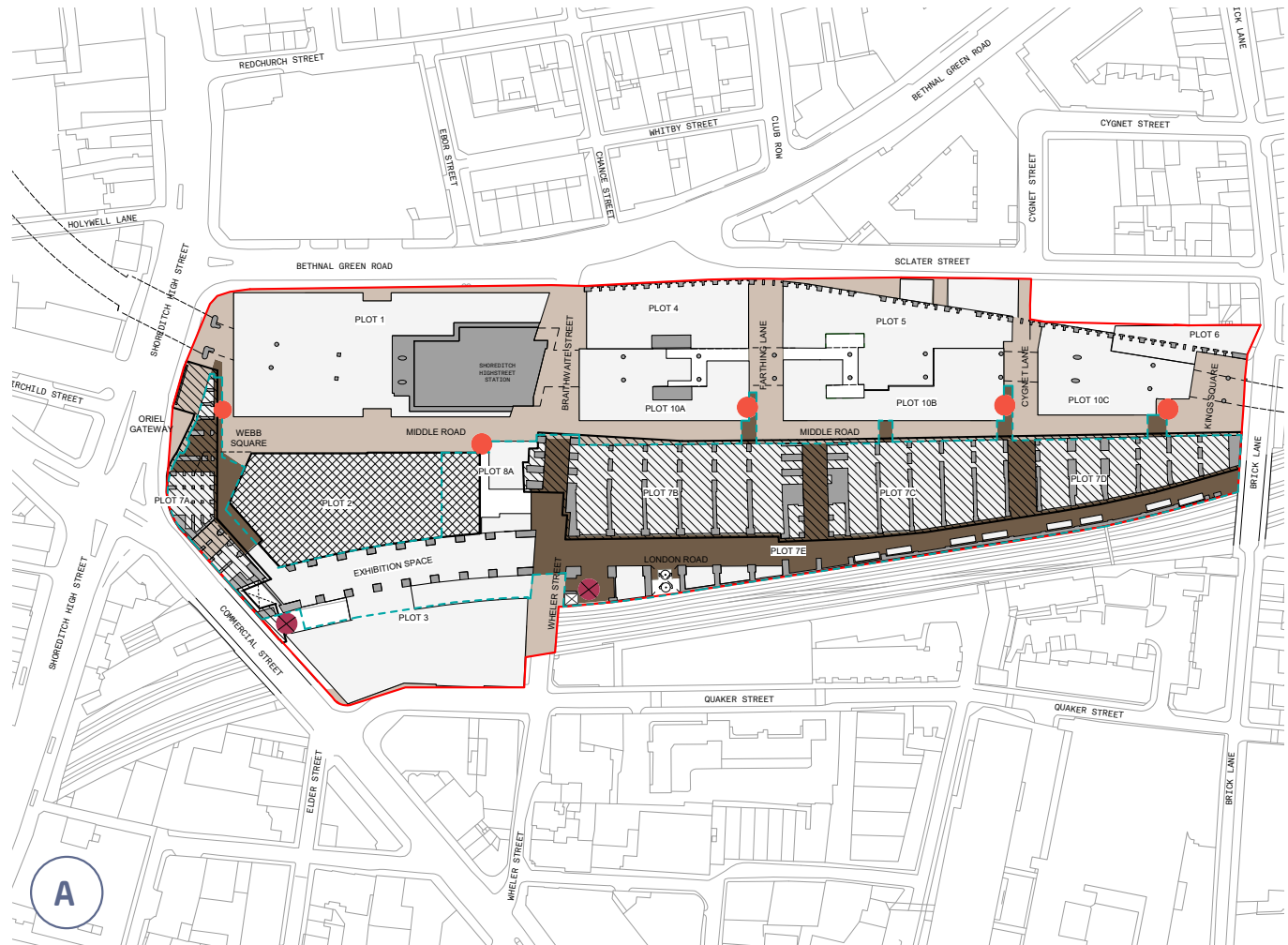


Fig 2.4.13: Minimum public realm areas - Ground

- |  |  |
|--|--|
| <span style="border: 1px solid red; display: inline-block; width: 10px; height: 10px;"></span> Application Boundary  | <span style="color: red;">●</span> Vertical Circulation  |
| <span style="border-top: 1px dashed black; display: inline-block; width: 10px; height: 10px;"></span> Building Extents Above (Projection)  | <span style="color: red;">⊗</span> Vertical Circulation (Gated)  |
| <span style="background-color: lightgrey; display: inline-block; width: 10px; height: 10px;"></span> Building Plots (Maximum Extents)  | <span style="background-color: #d3d3d3; border: 1px solid black; display: inline-block; width: 10px; height: 10px;"></span> Zone for Horizontal Bridges (Max Width 3m) |
| <span style="background-color: #808080; display: inline-block; width: 10px; height: 10px;"></span> Existing Retained Structure   | <span style="background-color: #800000; display: inline-block; width: 10px; height: 10px;"></span> Covered public realm area   |
| <span style="background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); display: inline-block; width: 10px; height: 10px;"></span> Full Details Submitted |  |
| <span style="background-color: #d2b48c; display: inline-block; width: 10px; height: 10px;"></span> Principal Public Realm Area   |  |
| <span style="background-color: #90ee90; display: inline-block; width: 10px; height: 10px;"></span> Private Garden  |  |

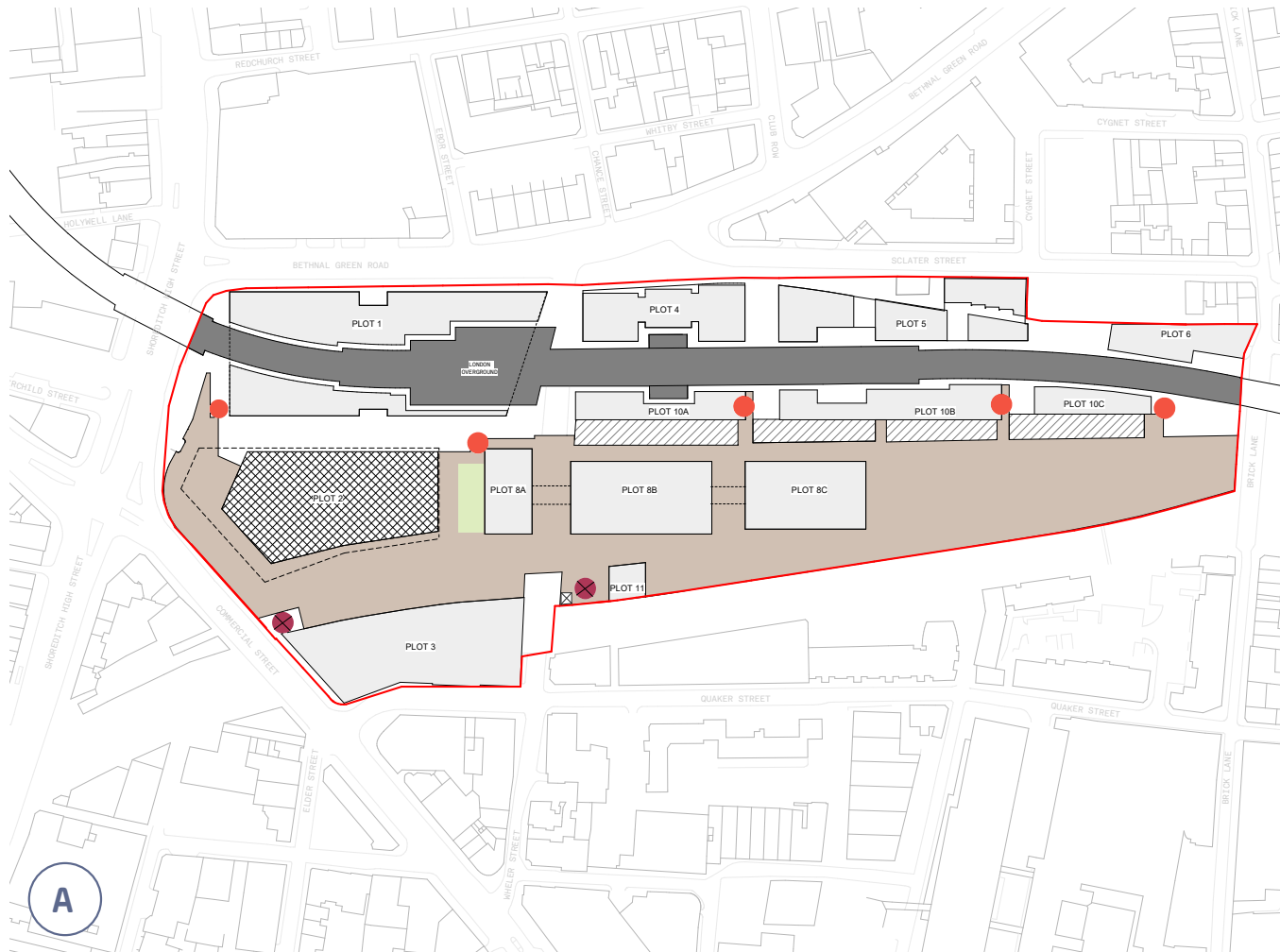
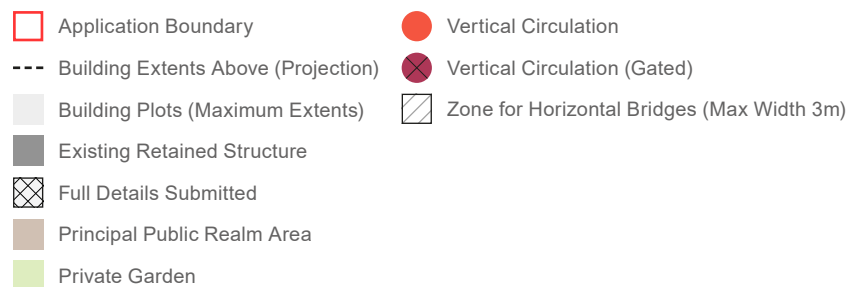


Fig 2.4.14: Minimum public realm areas - Platform



## 2.4.15 Minimum public realm areas - Platform: For Approval

The plan presents the proposed minimum areas of public realm at platform level.

For more detail see the transport assessment that accompanies the revised scheme submission.

The following specific issues should be noted:

- An area of private gardens (not publicly accessible) is provided between Plots 2 and 8.
- A zone for horizontal bridges has been indicated to provide access to Plot 10; the design, position and size of the bridges will be defined at reserved matters stage.

## 2.4.16 Pedestrian Access - Ground: For Approval

The plan presents the proposed main pedestrian access routes through the site.

For more detail with regards to pedestrian surveys, and comfort levels see the transport assessment that accompanies the revised scheme submission.

The following specific issues should be noted:

- A new east-west route links Shoreditch High Street and Brick Lane.
- Enhanced footway widths and new pedestrian routes created
- A new at grade pedestrian crossing will be provided north of Plot 1 across Bethnal Green Road.
- Pedestrian crossing improvements will be integrated as part of the Shoreditch triangle project.
- Access to the platform level is provided in seven locations; the access points that are proposed to close 'out of hours' as part of the crime reduction measures, are indicated on the plan with a cross hatch in the circle.

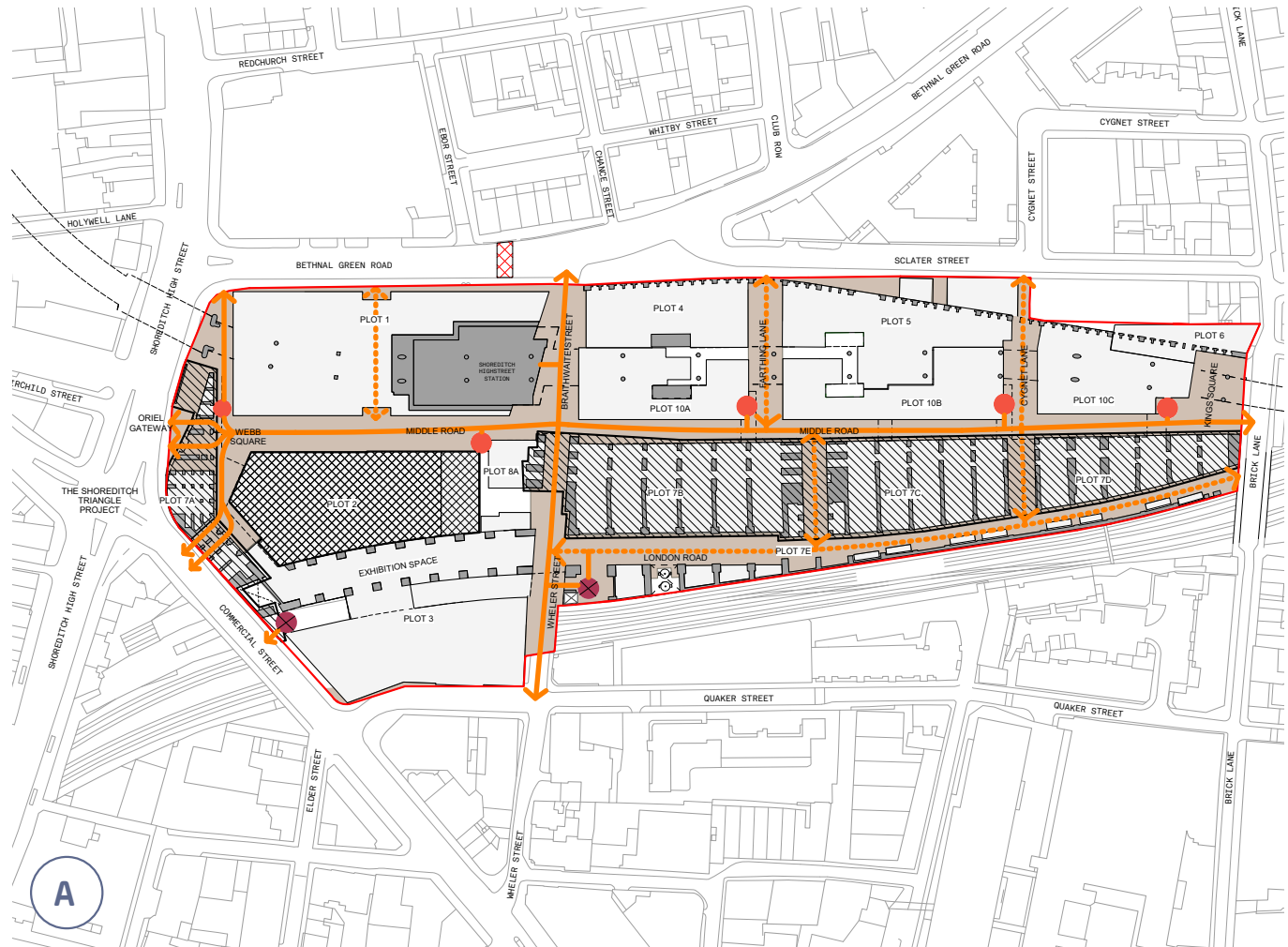


Fig 2.4.15: Pedestrian Access - Ground

Application Boundary	Full Details Submitted and LBA	Circulation (Primary)
Building Extents Above (Projection)	New Pedestrian Crossing	Circulation (Secondary)
Building Plots (Maximum Extents)	Principal Public Realm Area	
Existing Retained Structure	Vertical Circulation	
London Overground Line	Vertical Circulation (Gated)	
Full Details Submitted	Vertical Vehicle Service Access	



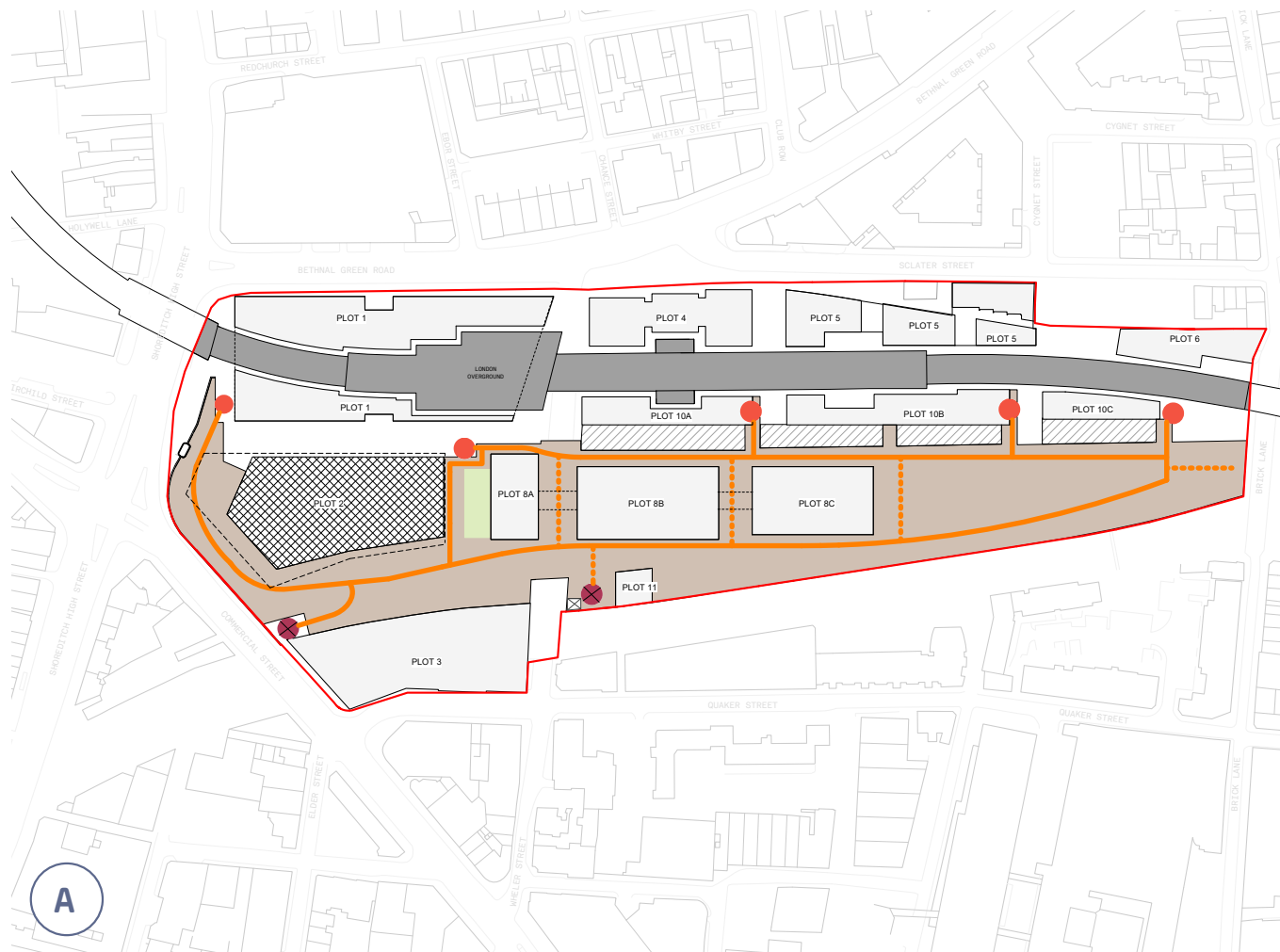
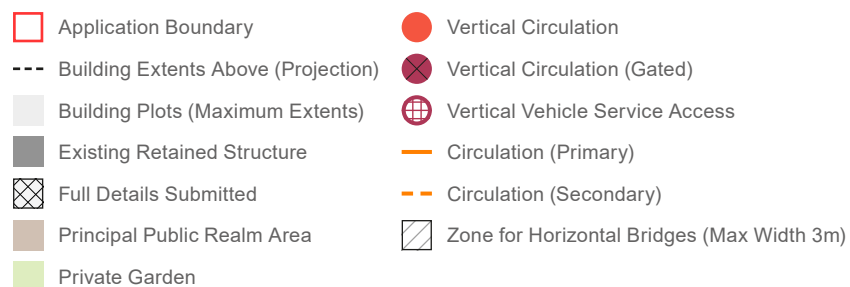


Fig 2.4.16: Pedestrian Access - Platform



## 2.4.17 Pedestrian Access - Platform: For Approval

The plan presents the proposed main pedestrian access routes through the site at platform level.

For more detail with regards to pedestrian surveys, and comfort levels see the transport assessment the revised scheme submission.

The following specific issues should be noted:

- The platform level creates a significant new area of accessible public open space.
- Seven vertical circulation points provide access to ground. These access points are served via a lift and stairs.

## 2.4.18 Proposed Levels - Ground: For Approval

The plan presents the maximum parameter footprint at ground level and proposes a finished floor level (FFL) for each building plot at above ordnance datum level (AOD).

The existing levels presented on the plan represent a summary of the site wide topographical survey.

The following specific issues should be noted:

- Levels indicated in red are proposed AOD levels within the application boundary.
- Levels indicated in black measured surveyed levels outside the application boundary.
- All spot levels are measured in metres above ordnance datum (AOD).
- Any specific deviations from these masterplan spot levels will be detailed within the landscape design guide.

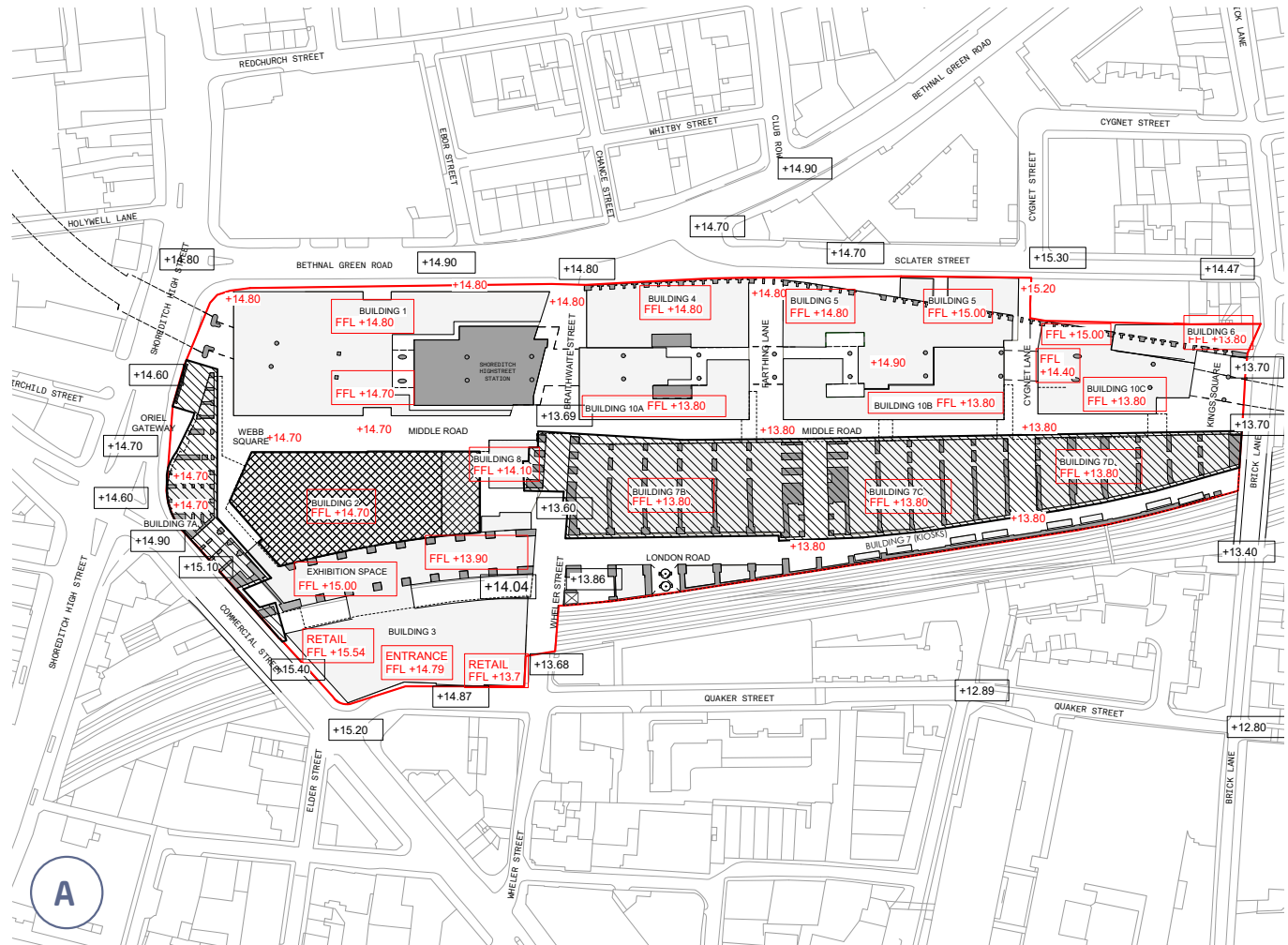
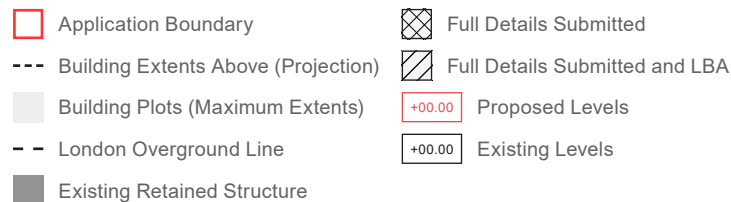


Fig 2.4.17: Proposed Levels - Ground



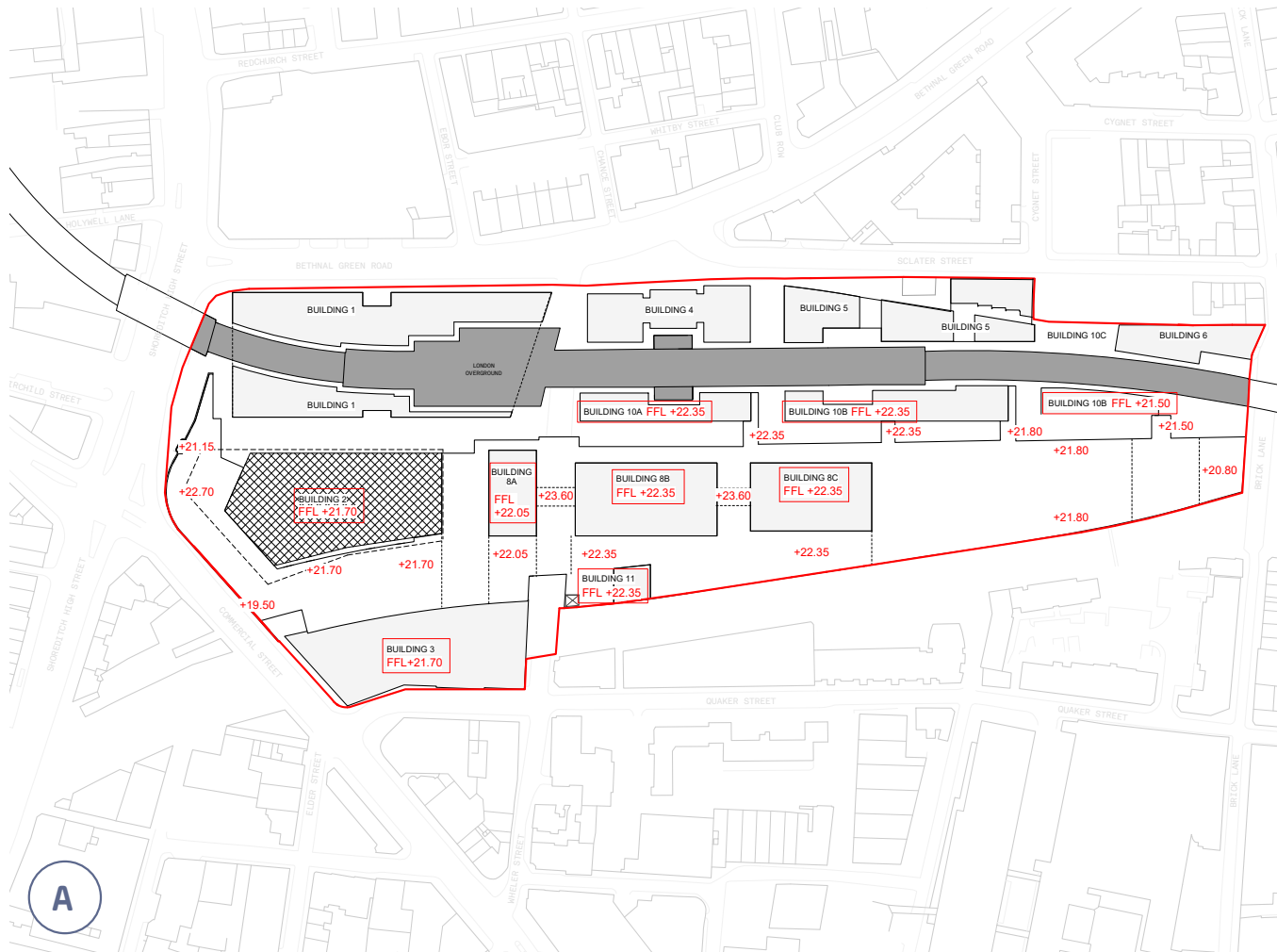


Fig 2.4.18: Proposed Levels - Platform



## 2.4.19 Proposed Levels - Platform: For Approval

The plan presents the maximum parameter footprint at platform level and proposes a finished floor level (FFL) for each building plot at above ordnance datum level (AOD).

The existing levels presented on the plan represent a summary of the site wide topographical survey.

The following specific issues should be noted:

- Levels indicated in red are proposed AOD levels within the application boundary.
- Levels indicated in black measured surveyed levels outside the application boundary.
- All spot levels are measured in metres above ordnance datum (AOD).
- Any specific deviations from these masterplan spot levels will be detailed within the landscape design guide.

## 2.4.20 Phasing - Ground: For Approval

The Goodsyard phasing strategy has been developed to ensure the feasibility of scheme delivery whilst ensuring best possible safety for all during construction, giving consideration to construction sequences that minimise disruptions to the local area.

The phasing plan assumes that the building, associated structures and landscaping will be delivered within the boundary identified.

Phasing is determined plot by plot by:

- Site Preparation phase
- Protection of heritage and rail assets
- Demolition phase
- Construction phase

The following specific issues should be noted:

- Phasing is subject to market demand and will need to be amended through time.
- Phasing presented here is subject to and will align with the ES volume 1, Chapter 5 'Demolition and Construction' report.

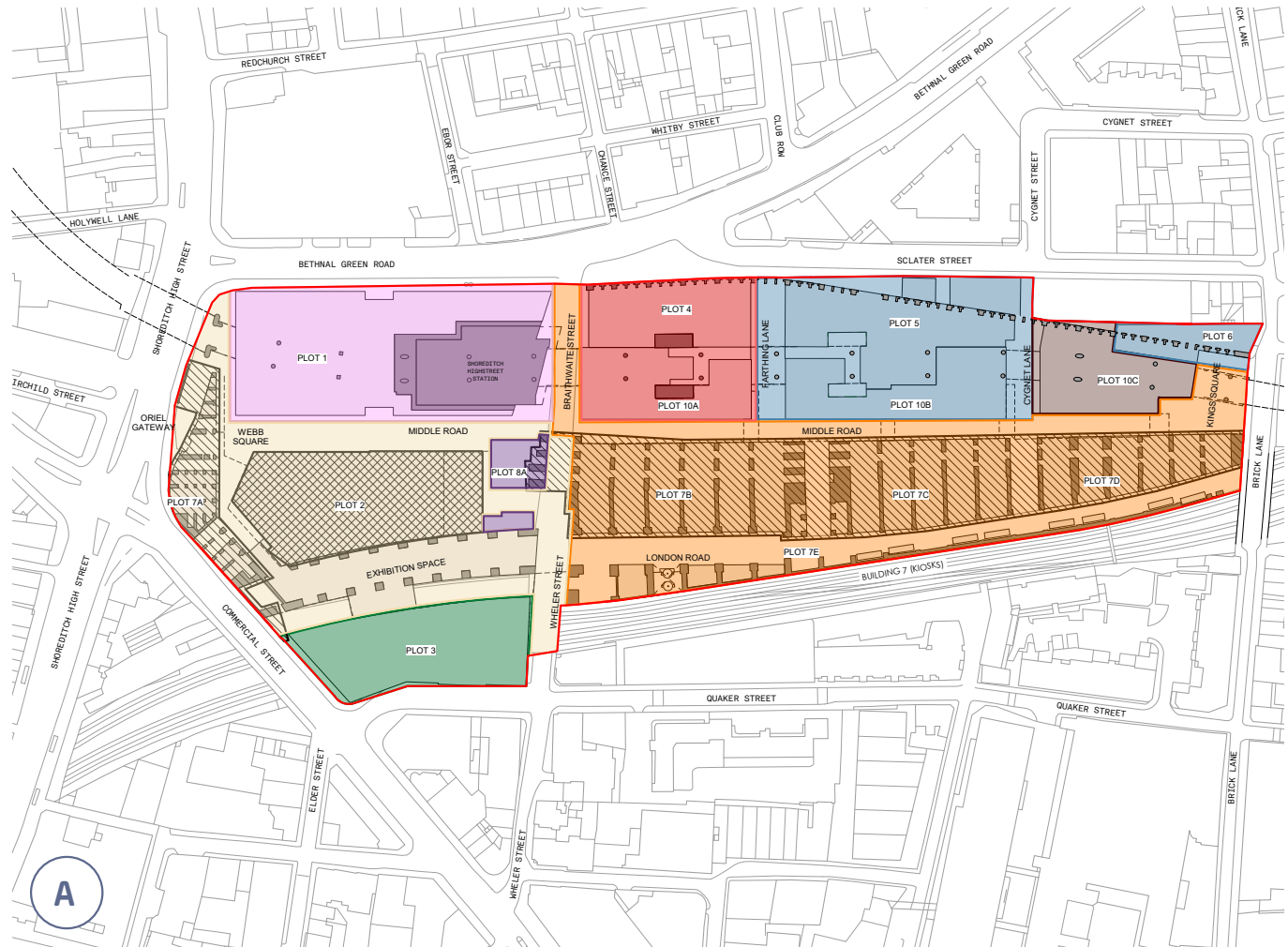
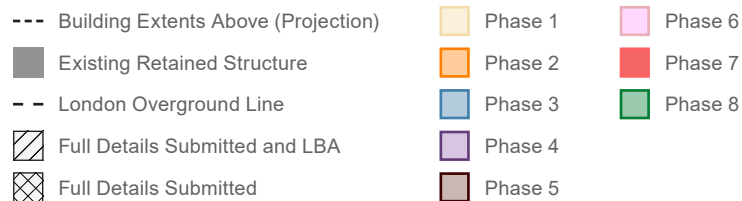


Fig 2.4.19: Phasing - Ground



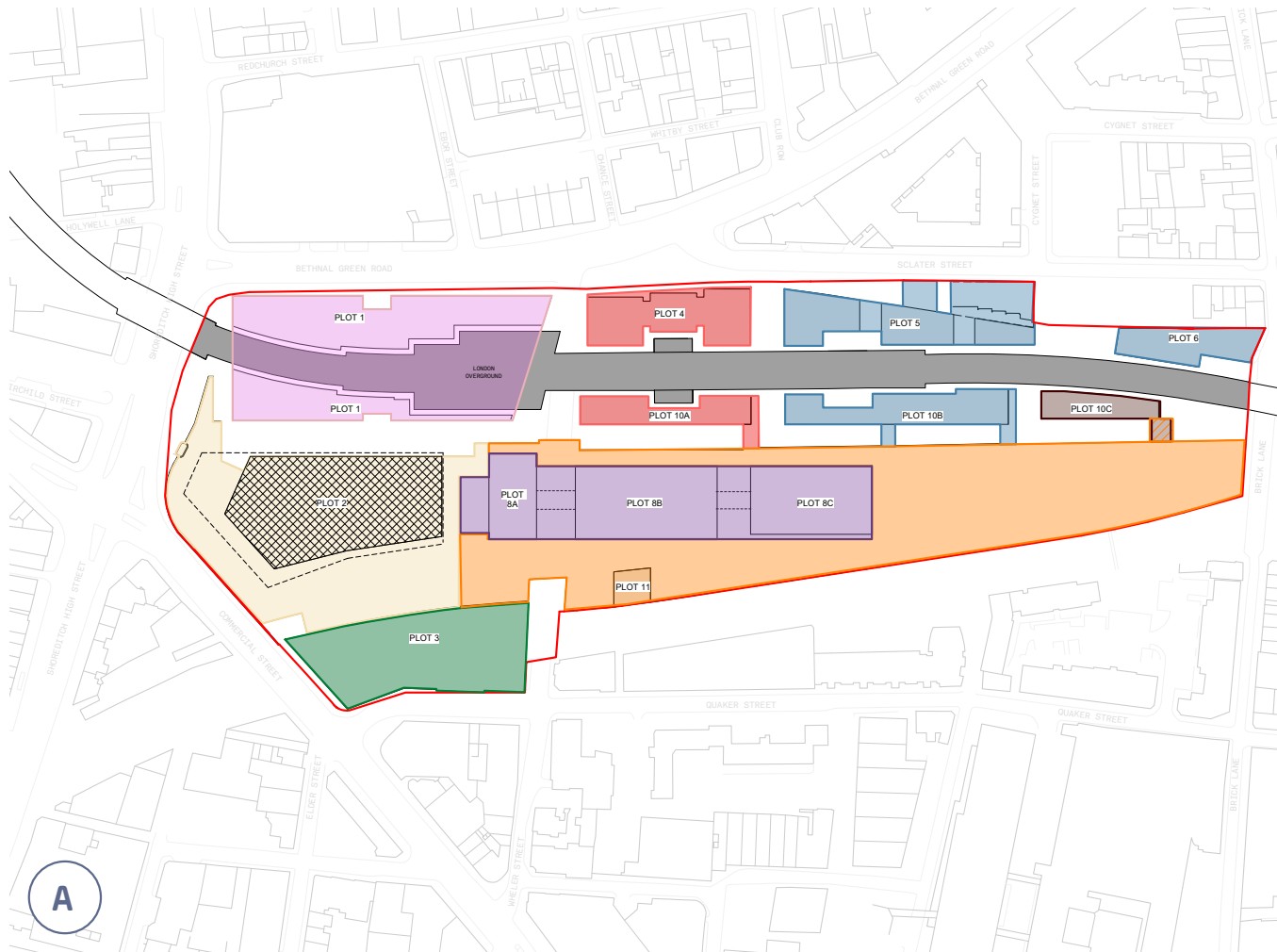
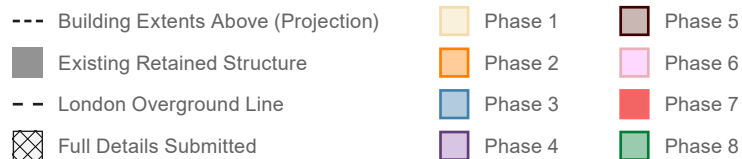


Fig 2.4.20: Phasing - Platform



### 2.4.21 Phasing - Platform: For Approval

The plan presents the platform level phasing plan.

The phasing plan assumes that the building, associated structures and landscaping will be delivered within the boundary identified.

Phasing is determined plot by plot by:

- Site Preparation phase
- Protection of heritage and rail assets
- Demolition phase
- Construction phase

The following specific issues should be noted:

- Phasing is subject to market demand and will need to be amended through time.
- Phasing presented here is subject to and will align with the ES volume 1, Chapter 5 'Demolition and Construction' report.



### 2.4.22 Frontages and Planning Use - Ground: For Approval

The plan presents the predominant use of frontage at ground level.

All plots seek to maximise the proportion of ground level elevations which form active frontage.

All plots will also seek to maximise the activity of mezzanine level elevations (where applicable).

All plots seek to minimise the impact of service entrances and ventilation intake/extract louvres.

Plot 2 and Plot 7 frontages are shown in detail within the Design and Access Statement and drawings submitted in full detail for these plots.

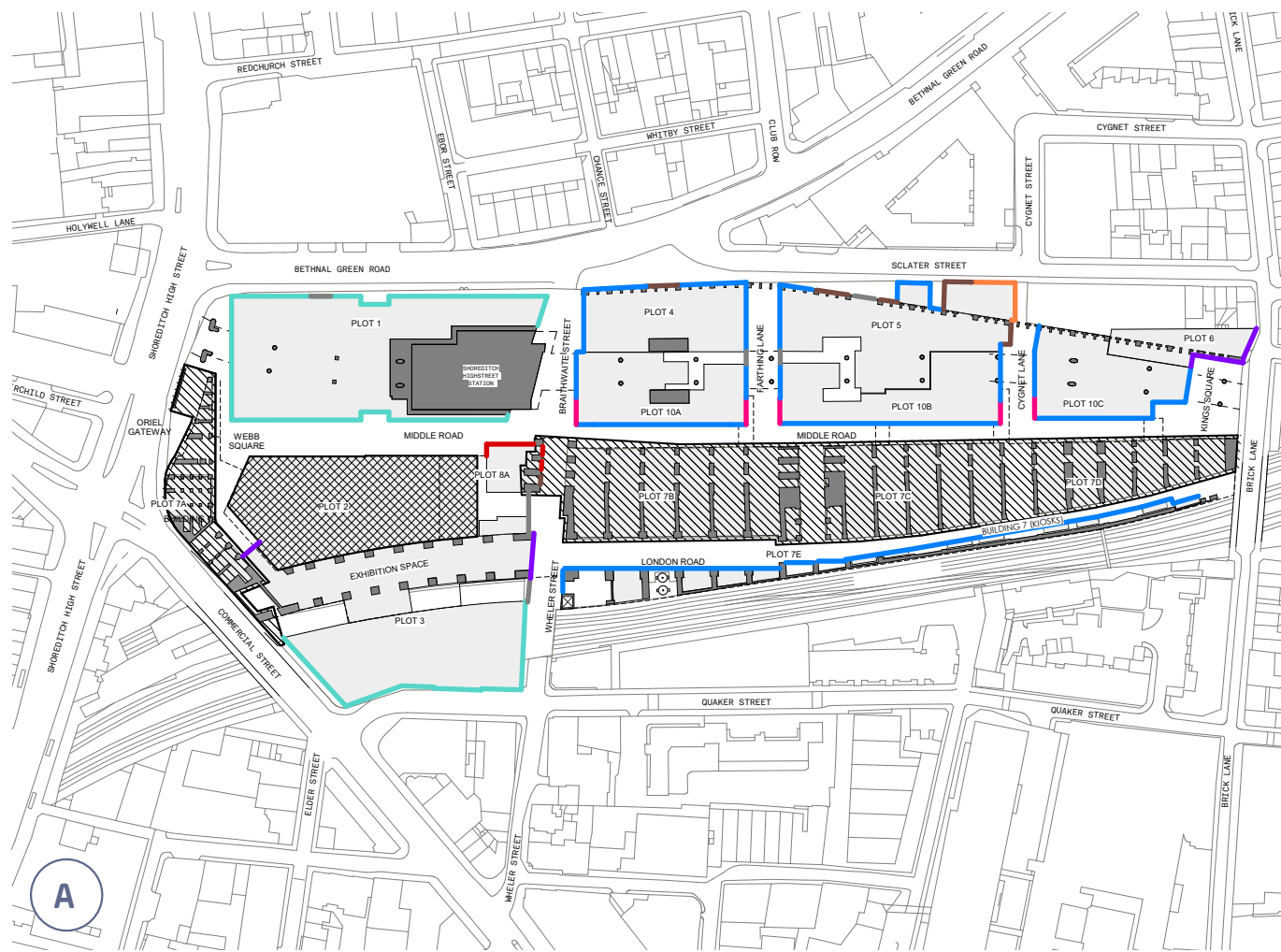


Fig 2.4.21: Frontages and Planning Use - Ground

- |   |              |                    |
|---|--------------|--------------------|
| --- Building Extents Above (Projection) | Use Class A  | Use Class D        |
| Building Plots (Maximum Extents)        | Use Class B  | Use Class A/B      |
| Existing Retained Structure             | Use Class C1 | Use Class A/C3     |
| --- London Overground Line              | Use Class C3 | Supporting Service |
| Full Details Submitted and LBA          |              |                    |
| Full Details Submitted                  |              |                    |

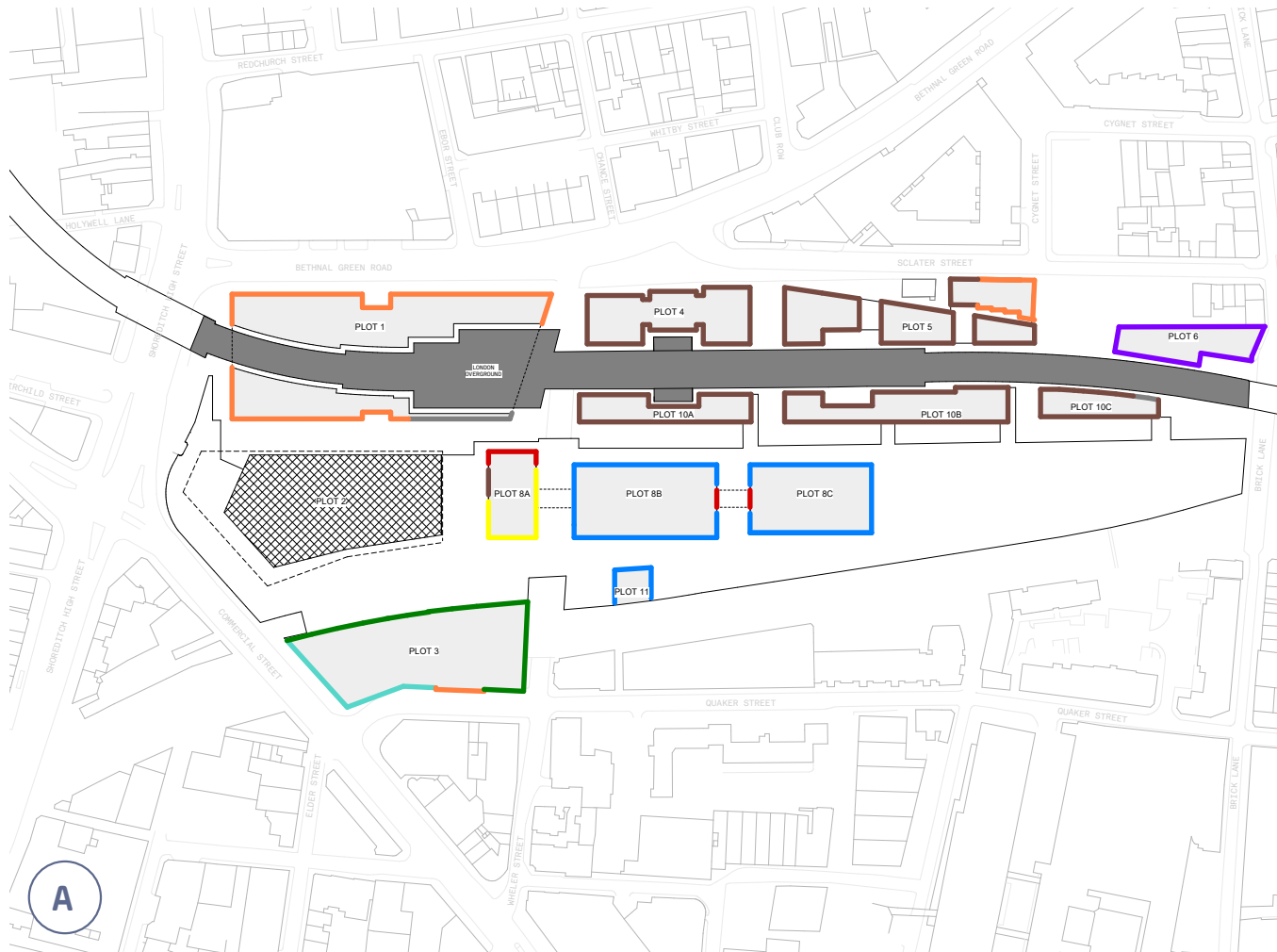


Fig 2.4.22: Frontages and Planning Use - Platform

--- Building Extents Above (Projection)	Use Class A	Use Class A/C1/D
Building Plots (Maximum Extents)	Use Class B	Use Class A/C3
Existing Retained Structure	Use Class A/B	Use Class A/B/D
- - London Overground Line	Use Class C1	Use Class D
Full Details Submitted	Use Class C3	Supporting Service

### 2.4.23 Frontages and Planning Use - Platform: For Approval

The plan presents the predominant use of frontage at platform level.

All plots will seek to maximise the proportion of platform level elevations which form active frontage

All plots will also seek to maximise the activity of mezzanine level elevations (where applicable).

All plots seek to minimise the impact of service entrances and ventilation grates.

Plot 2 and Plot 7 frontages are shown in detail within the Design and Access Statement and drawings submitted in full detail for these plots.

## 2.4.24 Frontages and Planning Use - Typical Upper: For Approval

The plan presents the predominant use of frontage at typical upper levels.

The typical upper floor levels represent the predominant use type throughout the building.

Plot 2 and Plot 7 frontages are shown in detail within the Design and Access Statement and drawings submitted in full detail for these plots.

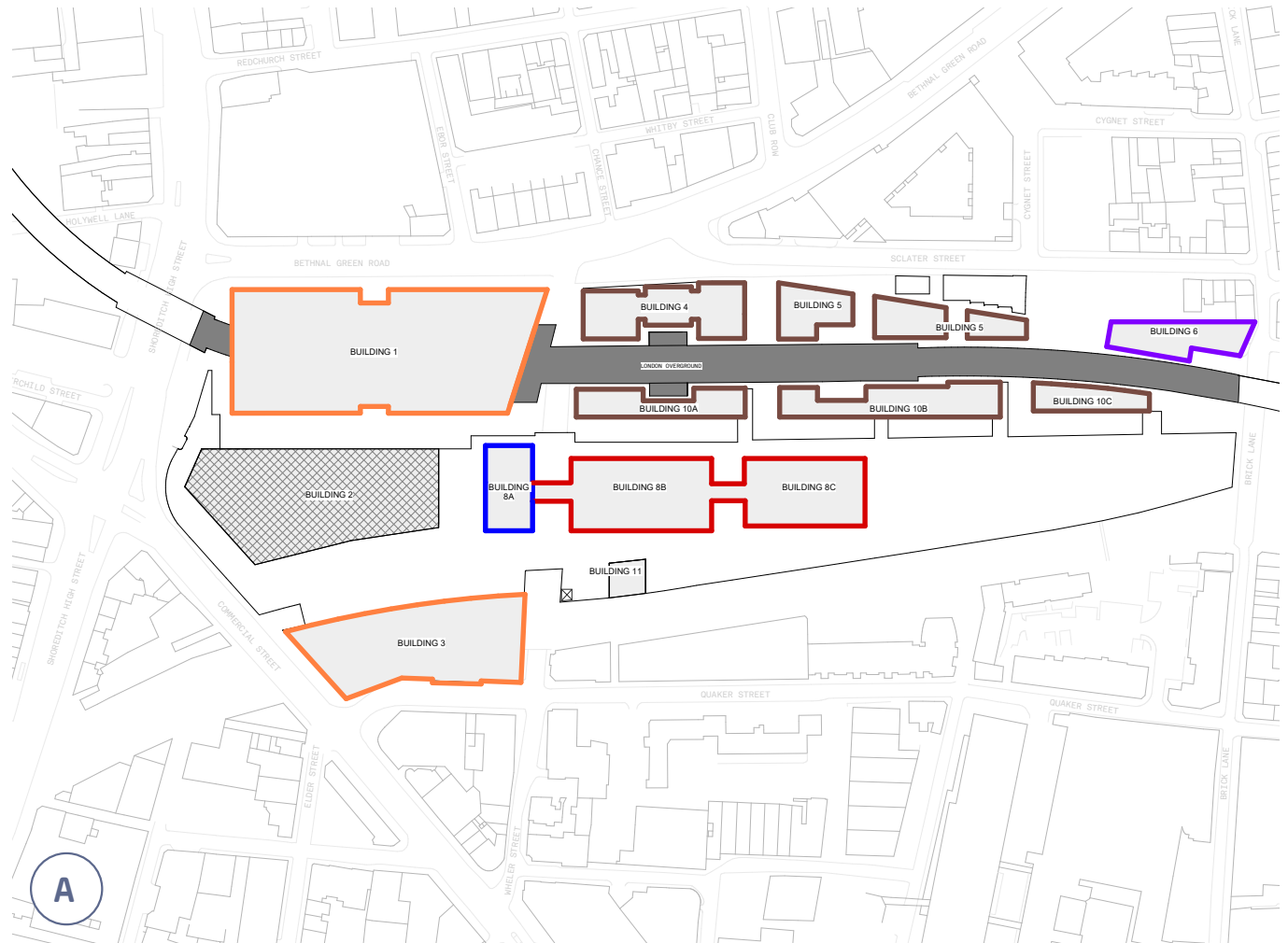


Fig 2.4.23: Frontages and Planning Use - Typical Upper

- |   |              |                    |
|---|--------------|--------------------|
| --- Building Extents Above (Projection) | Use Class A  | Use Class D        |
| Building Plots (Maximum Extents)        | Use Class B  | Use Class A/B      |
| Existing Retained Structure             | Use Class C1 | Use Class A/C3     |
| --- London Overground Line              | Use Class C3 | Supporting Service |
| Full Details Submitted                  |              |                    |

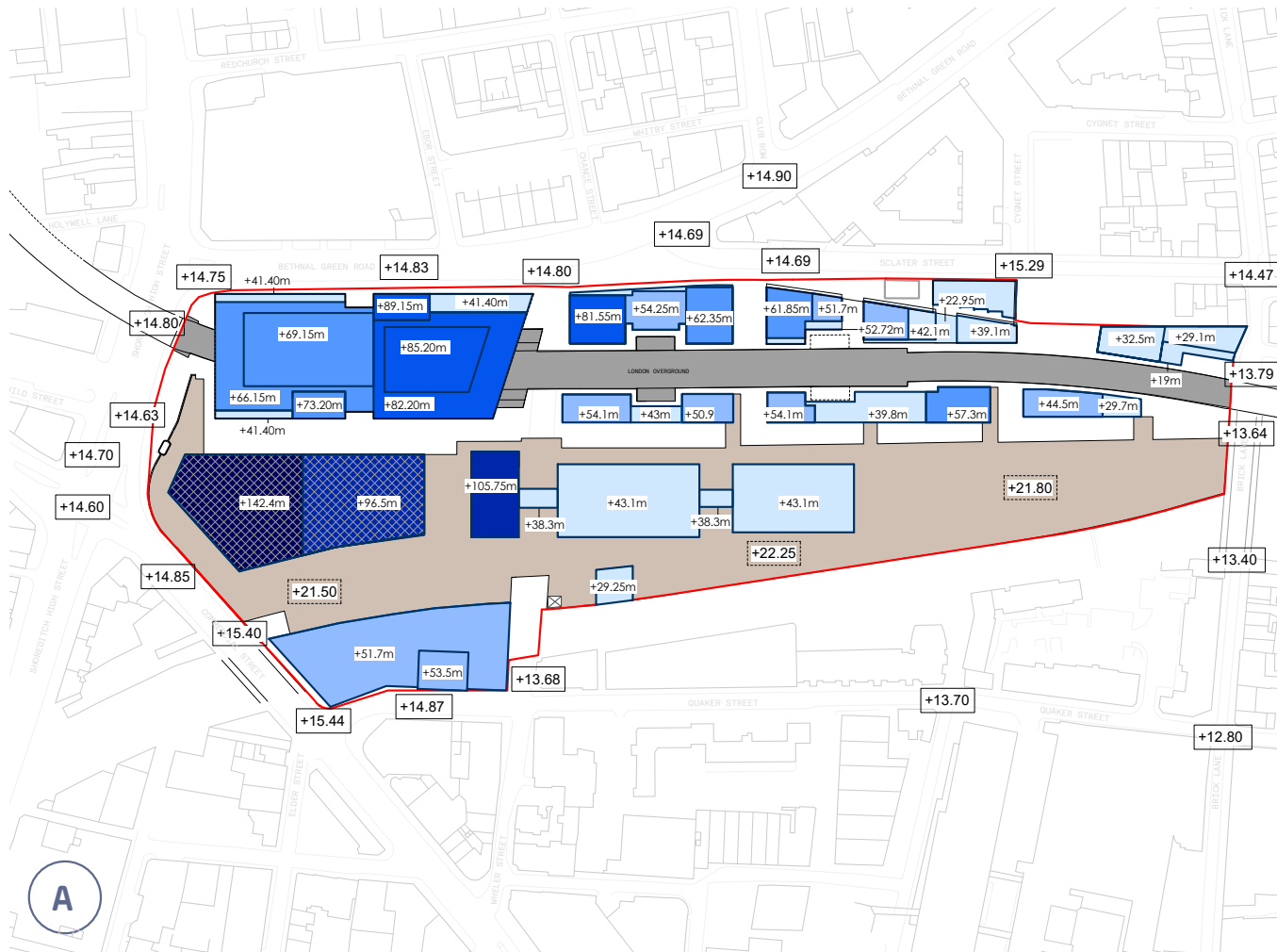
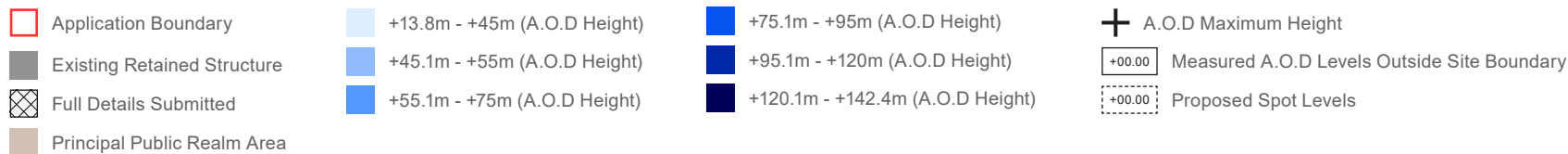


Fig 2.4.24: Maximum Building Heights - Plan



### 2.4.25 Maximum Building Heights - Plan: For Approval

The plan presents the maximum parameter building heights which are derived from the maximum building extents parameters.

Storey levels are illustrated through a range of blue shades running from light blue (lower) dark blue (higher).

Taller buildings are clustered to the West of the site and are located under the Hackney Shoreditch City Fringe tall Buildings strategy area. Smaller buildings are located to the East, adjacent to Brick Lane.

The following specific issues should be noted:

- Levels indicated on the building footprints are AOD levels
- Levels indicated in black measured surveyed levels outside the application boundary.
- All spot levels are measured in metres above ordnance datum (AOD).

## 2.4.26 Maximum Building Heights - Sections: For Approval

The Sections presented demonstrate the maximum parameter building heights which are derived from the maximum building extents parameters.

Storey levels are illustrated through a range of blue shades running from light blue (lower) dark blue (higher).

Taller buildings are clustered to the West of the site and are located under the Hackney Shoreditch City Fringe tall Buildings strategy area. Smaller buildings are located to the East, adjacent to Brick Lane.

The following specific issues should be noted:

- Levels indicated on the building footprints are AOD levels
- Levels indicated in black measured surveyed levels outside the application boundary.
- All spot levels are measured in metres above ordnance datum (AOD).
- Revised scheme proposed buildings not being cut in section are not coloured.

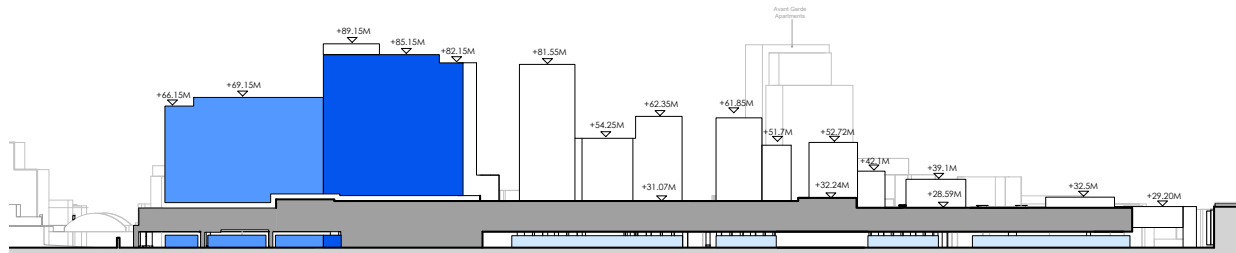


Fig 2.4.25: Section 1-1

PARAMETER SECTION 1-1  
1:750

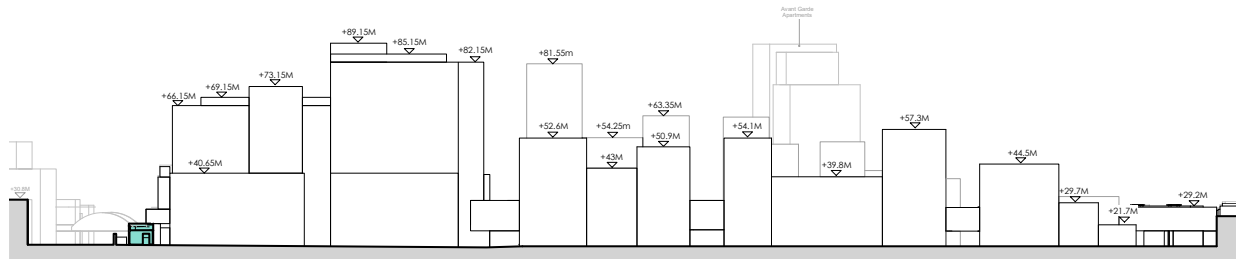


Fig 2.4.26: Section 2-2

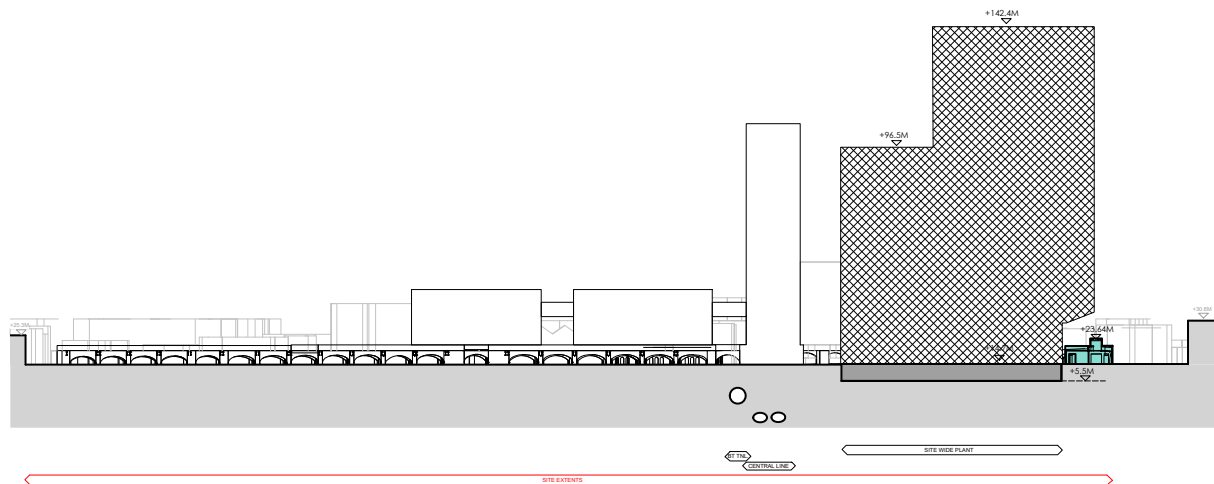


Fig 2.4.27: Section 3-3

- Application Boundary
- + A.O.D Maximum Height
- Existing Structure Renovation
- London Overground Train Box
- Full Details Submitted
- Basement



